

**AGENDA**  
**City of Hobbs Planning Board – Regular Meeting**  
**September 19, 2023 at 10:00 AM**

**W.M. “Tres” Hicks, Chairman**  
**Bill Ramirez**  
**Brett Drennan**  
**Larry Sanderson**

**Guy Kesner, Vice Chairman**  
**Ben Donahue**  
**Brett Clay**

**Tentative Agenda for the Planning Board Regular Session Meeting to be held on Tuesday, September 19, 2023 at 10:00 AM, at the City of Hobbs Annex Building, First Floor Commission Chambers located at 200 E. Broadway, Hobbs, NM 88240.**

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**AGENDA**

**Call To Order and Roll Call.**

- 1) Review and Consider Approval of Agenda.**
- 2) Review and Consider Approval of Minutes.**

**August 15, 2023 – Regular Meeting**

**Communications from Citizens. (3-minute limit per citizen)**

**ACTION ITEMS**

- 3) Review and Consider Final Plan for Meadowlands Unit II, located northwest of the intersection of Ponderosa and Ja-Rob, as submitted by property owner, Lemke Development, Inc.**

**DISCUSSION ITEMS**

**Adjournment.**

**The City will make every effort to provide reasonable accommodations for people with disabilities who wish to attend a public meeting. Please notify the City at least 24 hours before the meeting. Telephone 397-9232.**

*“Notice is hereby given that a quorum of the Hobbs City Commission may be in attendance at this meeting.”*

**PLANNING BOARD REGULAR MEETING  
MINUTES  
AUGUST 15, 2023**

The Hobbs Planning Board met on August 15, 2023, at 10:00 a.m. at the City of Hobbs Annex Building, First Floor Commission Chambers, located at 200 E. Broadway, Hobbs, NM 88240 with Mr. W.M. “Tres” Hicks, Chairman, presiding.

**Members Present:**

W.M. “Tres” Hicks, Chairman  
Guy Kesner, Vice Chairman  
Bill Ramirez  
Brett Clay

**Members Absent**

Larry Sanderson  
Ben Donahue  
Brett Drennan

Also present were members of the public and City staff as follows:

Todd Randall, City Engineer  
Anthony Henry, Deputy City Engineer  
Amber Leija, Assistant City Attorney  
Tim Woomer, Utilities Director  
3 Citizen(s)

Kevin Robinson, Development Director  
April Hargrove, Engineering Assistant  
Shawn Williams, Fire Marshal

**Call to Order and Roll Call**

Ms. April Hargrove, Engineering Assistant, did a roll call for members as follows:

Mr. Ramirez-yes, Mr. Sanderson-absent, Mr. Kesner-yes, Mr. Drennan-absent, Mr. Clay-yes, Mr. Donahue-absent, and Mr. Hicks-yes. There were four members present at the meeting.

Mr. Hicks called the meeting to order at 10:03 am.

**1.) Review and Consider Approval of Agenda**

The first item of business was to review and approve the agenda for the August 15, 2023 meeting. Mr. Hicks asked if there were any additions or changes to the agenda. Mr. Robinson stated Ms. Brenda Gomez was present at the meeting to discuss Item 7 of the agenda. Mr. Robinson suggested moving Item 7 before Item 3 of the agenda. Mr. Randall stated the ADA Transition Plan update, Item 6, is not complete and suggested moving it to Discussion Items. Mr. Ramirez made a motion, seconded by Mr. Kesner to approve the agenda as amended. The vote on the motion was 4-0 and the motion carried.

**2.) Review and Consider Approval of Minutes**

Mr. Hicks asked if everyone has had a chance to read the Regular Meeting Minutes from July 18, 2023.

Mr. Kesner made a motion, seconded by Mr. Ramirez to approve the Regular Meeting Minutes as presented. The vote was recorded as follows: Ramirez yes, Kesner yes, Clay yes, Donahue absent, Sanderson absent, Drennan absent, Hicks yes. The vote on the motion was 4-0 and the motion carried.

### **Communications from Citizens**

There were no communications from citizens.

### **Action Items**

- 7) **Review and Consider a proposed Subdivision located SW of the intersection of Woodfin & Linam within the ETJ.**

Mr. Robinson stated this is a proposed subdivision located southwest of Woodfin and Linam, within the ETJ. He stated Municipal Code 16.12 which is our summary subdivision procedures allows the administration to approve subdivisions, creating no more than three tracts that are fully 100% compliant with Municipal Code 16. The particular subdivision that's being proposed would not be compliant with MC 16.12. He further stated MC 16.12 also allows that same subdivision to be considered by the Board and the Commission as a standard plat. What that means is that it still has to be compliant, but there is a provision within the standard plat procedures for the bodies to grant variances. Mr. Robinson stated what makes this subdivision non-compliant is that Woodfin Drive has been dedicated. However, Lea County Ordinance Number 35 requires a two-step process to make that into a public road. He stated the County has accepted the dedication in the past, but in order for that to be a public right of way, the County has to accept the maintenance of the public roadway.

Mr. Robinson explained there is an excerpt on the plat, which shows City staff where it is located and if it's located adjacent to a public street. In this regard, Woodfin is not on the County map, meaning it is not a public street. He stated it is not a compliant subdivision. Additionally, staff would have to deal with the projection of Cochran in some form or another as per the rules and regulations of MC 16 which projects those roadways adjacent to it.

In response to Mr. Kesner's question, Mr. Robinson stated the Board has reviewed a sketch plan for this in the past. Mr. Robinson stated he was not sure if it included the two lots adjacent to Woodfin. He stated there is a full development that will project a new roadway southeast of these particular lots. He further stated these particular lots are not proposed to be subdivided within that subdivision.

Mr. Robinson clarified it is one undivided parcel and the property owner is trying to create two tracts.

Mr. Randall stated this road has some history. He stated there were improvements made to the roadway, but it wasn't done correctly and it was never accepted by an engineer record. Mr. Randall stated the roadway is too high and the County does not recognize or maintain it. He stated the right of way is dedicated. Mr. Randall further stated this is a claim of exemption. He explained the tract of land as far as splitting into two lots is exempt from the County subdivision regulations.

Mr. Randall stated the first issue is the roadway is not maintained by the County. The second issue is the projection of Cochran. Mr. Randall stated there is a 30-foot easement on the side, which would be the projection so the west property line at this location would be the side of the road. Additionally, Cochran is offset by at least 30 feet. Mr. Randall stated if we were to project Cochran it would encumber the property. He stated in the past, the Planning Board has approved vacating Cochran and other locations to the north. He further stated Grimes Street and Fowler Street were half-mile section roadways. We always talk about quarter-section roadways. Mr. Randall stated it is the same thing as far as Sockwell Dr. and Fowler St. He stated in this case we have Cochran, so at least there is a connectivity. Mr. Randall stated those are two reasons as far as why it cannot be approved this administratively, even though it's just one lot being split into two tracts and would meet our summary subdivision process.

Mr. Robinson explained staff cannot approve it under the standard. He stated there would have to be a variance that shows hardship and does not disregard all of our existing rules and regulations.

Mr. Hicks discussed the dimension of the property with the easement and stated the two lots might be too small to put septic and wells on.

Mr. Randall stated this is a claim of exemption and would only be a surface and subsurface easement.

Ms. Brenda Gomez, property owner, stated when the property was originally purchased, they considered splitting it with the east-west line rather than a north-south line to get the two-acre parcels. Ms. Gomez stated she spoke with the County and was told they couldn't do that because they couldn't use the south end of Cochran as a roadway because there was not enough right of way to use as access to the property. The access was only for the property just below to the south.

Mr. Robinson explained it would be compliant with the MC 16 if it was developed and Cochran was dedicated, north and south, then the lots created would be accessed from a public roadway.

In response to Mr. Hicks's question, Mr. Randall stated the proposed subdivision has not been



taken to the County because it is a claim of exemption.

In response to Mr. Kesner's question regarding the height of the road, Mr. Randall stated the County has a revised plan to redo Woodfin that is going to drop the road by 9 to 12 inches. Mr. Randall stated his understanding of the County's concerns is because of the height, with no provisions for water to get underneath the roadway, they don't want to take over maintenance for something that has not been certified by an engineer and may have drainage issues. Mr. Robinson stated the effect it has on the claim of exemption is that the County and the City do not recognize Woodfin Dr. as a public street.

Mr. Hicks discussed the International Fire Code (IFC) requiring two outlets for 30 units. He stated he does not see further subdivision of the proposed lots.

Mr. Kesner commented what would make sense is for there to be a fair share agreement between the City to pay 25%, the County to pay 25% and the developer to pay 50% to develop Fowler. He further said that what happens is the County and the City are going to have to develop Fowler to the property that is not inside the development. Mr. Kesner further said that long term, Fowler will need to be there for the flow from all of the developments that are taking place out there.

In response to Mr. Kesner's question regarding whether there are any parcels adjacent to the City and if the property can be annexed inside city limits, Mr. Randall stated the property cannot be annexed. Mr. Robinson stated the developer is proposing to extend municipal infrastructure in the form of a water line, which brings us into the fair share.

Ms. Gomez commented unless there is enough of a fair share agreement, it is not feasible. Ms. Gomez stated if the lots are not split, they are still going to build a house on the property. It's just whether they build one or two.

Mr. Hicks suggested putting a cul-de-sac north of the property or putting access from the two blocks provided and rearranging the spacing to where they have access to Cochran to the south.

Mr. Robinson explained any lots that are created must have access from a dedicated developed public street. He further explained if that access occurs within the boundaries of the property, it becomes the developer's responsibility to install, develop, dedicate, and have the county accept maintenance of the new street.

Mr. Hicks suggested approving the subdivision contingent upon a master plan that connects the street to Fowler and the City and County would have to get together on that.

Mr. Kesner expressed his concern and stated if there is not a hardship then the Board cannot

accept it and allow it to be a subdivision.

Following a lengthy discussion, Mr. Ramirez made a motion to approve the subdivision contingent upon Woodfin Dr. being County maintained or there being a development agreement accepted by the County, seconded by Mr. Kesner and the motion carried 3-1, with Mr. Kesner opposing.

**3) Review and Consider FY 2023-2024 Market Rate Multi-Family & Single-Family Housing Municipal Infrastructure Reimbursement Incentive & Public Participation Infrastructure Extension Development Agreement Policy.**

Mr. Robinson stated this is the Fiscal Year Market Rate Multi-Family and Single-Family Housing Municipal Infrastructure Reimbursement Incentive and Public Participation Infrastructure Extension Development Agreement Policy. Mr. Robinson stated the municipality has been participating in development agreements, incentivizing the production of certain housing that the public has decided that we are short on. He stated it would be Single-Family Housing, Multi-Family Housing, and especially the Low-Income Housing Tax Credits. He stated in the Low-Income Housing Tax Credit, the state statute states it is an exception to the anti-donation rule and provides you do everything this way. Mr. Robinson explained you're allowed to give funds that would go back to the straight production of the house units. He further explained that the same provision does not extend to multifamily and single-family homes; however, incentivizing the production of those units by offering them reimbursement of the public infrastructure adjacent to those structures has been a way that we have allowed the public to increase our number of units. Mr. Robinson stated the policy is presented to the Board on a yearly basis.

Mr. Randall stated the only difference on page one is that the multi-family rate has been reduced to \$30,000. The other thing that is being introduced and has been a common practice is the gap infrastructure is paid 100% when inside the city limits. Also, the minimum participation has been dropped to \$5,000.00 because there have been some small gaps or oversize that didn't qualify because originally it was \$50,000.00.

In response to Mr. Ramirez's question, Mr. Randall stated the unit numbers are over a decade old. He also stated the unit prices were cut by 50%. Mr. Randall explained three years ago, it was \$10,000 per lot. These were doubled as far as a fair share of 50%, but our incentive program with HB6 the Commission approved to reduce that from \$10,000.00 to \$5,000.00 per lot. Mr. Randall stated the current budget is about \$400,000.00 to continue the program. He stated there is a separate line item for oversize or gap infrastructure improvements.

Mr. Robinson explained those are the budgeted numbers as of today, but if the project funds run out we can request a budget adjustment.

Mr. Hicks asked if the numbers have been raised for inflation or the current market. Mr. Randall said they have not.

Mr. Alberto Caballero, a developer, commented about the participation amounts being too low for the current market rates. Mr. Caballero discussed how much the prices have gone up on materials.

Mr. Randall clarified it is a participation contribution and is not necessarily going to represent 50% of what those costs are or even 25% of at today's prices.

In response to Mr. Kesner's question, Mr. Randall explained as far as an eight-inch waterline in a subdivision multiply that times four. He stated the City uses \$50.00 per linear foot for the participation agreements. In the past, it was up to \$10,000.00 and that was at \$25.00 per linear foot. He further explained when the recommendation was to cut those numbers in half again to go from \$10,000.00 to \$5,000.00 per lot the housing incentive unit prices were reduced to \$12.50.

In response to Mr. Ramirez's question, Mr. Randall stated the Planning Board can make direction as far as housing incentive recommendations to the City Commission. He stated the Mayor is an advocate for the program and wants to ensure that the program continues. Mr. Randall stated that was one of the changes as far as from the \$10,000.00 per lot to \$5,000.00 per lot and what staff did as far as in order to calculate that we just reduced those unit prices by 50% as well, and that's where you get to the \$12.50

Mr. Hicks asked if it makes any sense to increase the per-foot numbers? Let's just say we wanted to double them to be more consistent without increasing the per-unit numbers. Mr. Randall explained that it is still going to cap every subdivision at \$5,000.00.

Mr. Hicks stated he was just trying to figure out if it made sense to maybe increase the profit incentive by 100% and increase the per lot incentive by 50%. In other words, go from \$5,000.00 to \$7,500.00 per lot. Mr. Randall stated if that's the direction the Planning Board wants to make, we're going to be fairly consistent with the current policy and that's going to continue until we bring something back to the Board. He stated if the Planning Board wants to increase this and come back to this as far as the itemized prices for infrastructure so that the Board has those numbers, then that may be a better conversation. Mr. Randall stated he can use that at least historically for what we've participated in and what kind of fiscal impact that's going to have as well.

Mr. Hicks asked if we are spending all of our budget every year on this? Mr. Randall said yes. He stated every preliminary budget we make assumptions and we make cuts. He stated there were significant cuts this last year.

Mr. Hicks stated housing is very important to our community and if we can help with that without getting overboard and try to figure out if changing the policy is going to have any effect on the actual use of the incentive program. Mr. Randall stated it has been a great marketing tool. He stated most communities do not offer this incentive. He stated the only reason we went from \$10,000.00 a lot and \$5,000.00 a lot was because we were concerned that we were going to run out of money. And then once the incentive policy stopped, it was going to be difficult to get it back up. Mr. Randall commented about the Board needing more information

as far as how much has been spent every year. What the trend is as far as the housing incentive? What do we project as far as future revenues? How much we can dial this up to create the keep the program? Also, to recognize increased inflation from our developers, right? Todd stated that is something he couldn't present or answer the questions at the moment, but he would be able to at the next meeting.

Mr. Kesner made a motion to table this item, seconded by Mr. Clay. The vote on the motion was 4-0 and the motion carried.

Mr. Hicks stated he just wants to have a good basis for recommendation to the City Commission.

**4) Review and Consider a Fair Share Development Agreement for the extension of complete public infrastructure (Water, sewer, street, curb, and gutter) within the Roth Road ROW.**

Mr. Robinson stated this is the Fair Share Development agreement for the extension of complete public infrastructure (water, sewer, street, curb, and gutter) within the Roth Road right of way.

Mr. Robinson stated this is the projection of Roth Road north of Navajo for a distance of +/- 235 linear feet. He stated it includes fair share participation of all of the infrastructure located on the west side of the centerline which would be the full water, water, sewer, asphalt, curb, gutter, and everything except for the sidewalk.

Mr. Randall stated the property owner is 100% responsible for his side of the roadway and the City is deferring the sidewalk on the west side.

Mr. Robinson stated staff calculated for this fair share development agreement not to exceed \$42,300. Mr. Kesner asked if the total cost is \$84,600 for the whole extension. Mr. Randall stated that is our participation for half of the waterline, sewer line, and half the roadway at \$42,300. He stated it does go back to the housing incentive numbers.

Mr. Hick's asked if it is going to be developed as a commercial property and not a housing development. He also asked if the infrastructure was going to be extended to areas that are residential and provide access and connectivity through there. Mr. Randall said that is correct. Mr. Hicks stated he just wants to make sure that there is rationale for doing it.

Mr. Randall explained this has been developed as a commercial property and it's under construction currently. He stated the access would be off of the new roadway. It is a full roadway extension similar to the existing radiuses on Navajo. He stated it would not be a 41-foot back-to-back roadway. It's only a 37-foot back-to-back roadway because this is probably the only commercial property and then it turns into residential.

In response to Mr. Hicks's question, Mr. Robinson stated the policy is geared toward minor and major collector roadways. He explained if you're developing a piece of property and it's adjacent to a dedicated roadway that has not been developed, it's your job as a property owner or developer to develop the entirety of that roadway. Mr. Robinson stated we are viewing this as an incentive to develop those premature roadways that the municipality has in existence and to improve them. He stated that we are incentivizing the production of the commercial property by offering a Fair Share development agreement.

Mr. Hicks asked if developers will have to pay their fair share if they develop on the west side of Roth Road. Mr. Randall explained that the more incentive that we give to the developer, the higher the assessment is for the adjacent property owner.

Mr. Ramirez made a motion, seconded by Mr. Kesner to approve the Fair Share Development agreement for the extension of the public infrastructure within the Roth Road right of way. The vote on the motion was 4-0 and the motion carried.

**5) Review and Consider an Infrastructure Oversize and Over Depth Development Agreement for public infrastructure within the Del Norte Parkway ROW (west of Grimes).**

Mr. Randall stated this is an Infrastructure Oversize and Over Depth Development Agreement for public infrastructure within the Del Norte Parkway right of way (west of Grimes). Mr. Randall stated this is an oversize for Del Norte Parkway from a 41-foot roadway to a 61-foot roadway and also increased pavement thickness for an estimated \$25,000. He stated there is an existing waterline, but the size of the sewer line being installed is a 10-inch waterline so it doesn't qualify for any additional cost share. He stated we are estimating about \$25,000 for the oversize of that Roadway Section, a little over 275 feet. He further stated the reason it's not a 50/50 in this case is because the property owner owns both sides. Mr. Randall stated this is Plaza One and Plaza Two of Kassis Development.

In response to Mr. Hicks's question, Mr. Randall stated the roadway does line up with Del Norte. Mr. Randall explained that the roadway was widened to 61 feet in order to introduce left turn lanes, through lanes, and a free right turn lane. He explained as it continues into Del Norte it will go back down to a 41-foot roadway. Mr. Randall stated the intersection is a half-mile roadway and it does line up with the existing roadway.

Mr. Randall stated it is an estimated \$25,000.00 because there are no testing results on the roadway to ensure that there are actually three inches of asphalt, so coring is going to be a requirement.

Mr. Robinson mentioned that the oversized land with oversized pavement thickness could be crucial in the future as density increases on the property.

Mr. Clay made a motion, seconded by Mr. Ramirez to approve the Infrastructure Oversize and Over Depth Development Agreement for public infrastructure within the Del Norte Parkway

right of way (west of Grimes). The vote on the motion was 3-0 with Mr. Kesner abstaining and the motion carried.

Mr. Kesner explained his abstention is due to his association with Del Norte. He stated this property was sold by Del Norte and 50% of it is owned by his employer, Daniels Insurance.

### **Discussion/Updates**

#### **6) Review and Consider an ADA Transition Plan Update (Infrastructure within Public Right of Way).**

Mr. Randall stated the City is required to have an ADA transition plan. The last transition plan was done in 2011. He stated in September the City has to update the plan in order to be eligible for federal funds. Mr. Randall stated this ADA Transition Plan is only associated with the public right of way and doesn't include public facilities, but we are working on a plan for that as well. He stated the City is using existing guidelines. Mr. Randall stated the Public Right of Way Accessibility Guidelines ProAct was adopted in the Federal Registry as of August 8, 2023. He further stated in 1991 they adopted the ADA guidelines, tied to the Act that addresses the subject of making public services and public transportation accessible to those with disabilities. Mr. Randall stated at a minimum the City has to do an assessment of all of the curb ramps. He stated the committee wants to ensure every roadway intersection barrier will be removed. He further stated phase two, which hasn't been done, is the sidewalk assessments and then phase three is also assessing all of the right of way limitations, any obstructions or constraints within our right of way in order to make all of the roadways ADA accessible.

Mr. Randall explained the scope of work is to use our GIS system to do analysis as far as what ramps we have and what barriers we have. He further explained there are an estimated 3,400 ramps or needed ramps within the City. Mr. Randall stated future evaluations are also going to include the sidewalks, driveways, parks, and established cost estimates. Mr. Randall explained the reason this is being as a discussion and not final is because we are still doing an analysis and it was not complete prior to this meeting. He explained just a rough estimate in 2011 just to put an ADA ramp at every intersection was going to be close to \$6 million in the entire City.

Mr. Randall explained the way we prioritize is if it is a funded project we are required to make those ADA compliant. We are focused on where there are arterial roadways, where everybody is driving and walking, and the collector roadways. He stated once we start getting into the residential areas where there is no sidewalk it becomes a very low priority. He further explained the challenge is there are areas with limited right of way and there are areas with lots of obstructions.

Mr. Robinson commented on how the developments are compliant with the subdivision rules and regulations. He stated that City staff has done an excellent job of requiring the necessities during the developmental or subdivision stage. He stated the bulk of the \$6 million improvements that are being discussed were done before the regulations existed.

In response to Mr. Hicks's question regarding the City maintaining the sidewalk, Mr. Randall explained the City has an incentive to repair sidewalks mainly in residential areas. He stated the City offers to pay 50% of the cost. Mr. Randall explained the sidewalk has to be elevated or cracked, and then it has to be repaired with today's standards. He further explained the City will replace those portions of sidewalks on the arterial roadways and collector roadways because that is a priority as far as making sure we maintain that connectivity. Mr. Randall stated the City has a 50/50 program. He stated the budget is about \$10,000 annually for the program.

Mr. Randall stated the ADA Transition Plan Update will need to be finalized then it will go to the Commission for approval.

### **Adjournment**

With nothing further to discuss Mr. Ramirez made a motion, seconded by Mr. Kesner to adjourn. The meeting adjourned at 11:45 a.m. The vote on the motion was 4-0 and the motion carried.

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W.M. "Tres" Hicks, Chairman

**September 19, 2023**  
**Planning Board Regular Meeting**

- 3) **Review and Consider Final Plan for Meadowlands Unit II, located northwest of the intersection of Ponderosa and Ja-Rob, as submitted by property owner, Lemke Development, Inc.**







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GENERAL NOTES:

1. ALL IMPROVEMENTS, UNLESS OTHERWISE MODIFIED IN THE PROJECT SPECIFICATIONS, SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APWA NEW MEXICO STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION, EXCEPT WHERE OTHERWISE NOTED IN THE DRAWINGS.

2. STANDARD DRAWINGS: REFER TO STANDARD DRAWINGS FOR THE NEW MEXICO STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, LATEST EDITION UNLESS OTHERWISE NOTED.

3. THE CONTRACTOR AGREES THAT HE/SHE SHALL ASSUME THE SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS DURING THE COURSE OF PROJECT CONSTRUCTION, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS, AND THE CONTRACTOR SHALL DEFEND, INDEMNIFY AND HOLD THE OWNER AND ENGINEER HARMLESS FROM ANY AND ALL LIABILITY, REAL OR ALLEGED, IN CONNECTION WITH THE PERFORMANCE OF WORK ON THIS PROJECT, EXCEPTING FOR LIABILITY ARISING FROM THE SOLE NEGLIGENCE OF THE OWNER OR ENGINEER.

4. NO CHANGES SHALL BE MADE TO THESE PLANS WITHOUT THE WRITTEN APPROVAL OF THE OWNER, ENGINEER AND ALL APPROVAL SIGNATORIES. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR CONSTRUCTION METHODS OR TECHNIQUES OR FOR THE PROSECUTION OF THE WORK AS SHOWN ON THESE PLANS. THE ENGINEER SHALL NOT BE RESPONSIBLE FOR THE ACTS OR OMISSIONS OF THE CONTRACTOR, SUBCONTRACTORS OR OTHER PERSONS PERFORMING ANY OF THE WORK OR FOR THE FAILURE OF ANY OF THEM TO CARRY OUT THE WORK IN ACCORDANCE WITH CONTRACT DOCUMENTS.

5. UNLESS OTHERWISE PROVIDED AS PART OF THE CONSTRUCTION PLANS, A COMPLETE TRAFFIC CONTROL PLAN SHALL BE PREPARED BY THE CONTRACTOR WHEN ANY PORTION OF THE WORK IS IN THE PUBLIC RIGHT-OF-WAY OR AFFECTING ON-SITE VEHICLE OR PEDESTRIAN CIRCULATION. ALL CONSTRUCTION SIGNING, BARRICADING AND CHANNELIZATION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) LATEST EDITION. THE PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL FOR ON-SITE CONSTRUCTION. TRAFFIC CONTROL WITHIN THE CITY/COUNTY RIGHT-OF-WAY SHALL DEFAULT TO THE CITY/COUNTY TRAFFIC DEPARTMENT FOR APPROVAL. THE CONTRACTOR SHALL NOT IMPLEMENT THE TRAFFIC CONTROL PLAN UNTIL APPROVAL OF THE PLAN HAS BEEN RECEIVED FROM THE ENGINEER. THE TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO AND APPROVED BY THE CITY OR ENGINEER.

6. THE CONTRACTOR SHALL DESIGNATE AT LEAST ONE EMERGENCY CONTACT PERSON, AND SHALL PROVIDE TELEPHONE NUMBERS WHERE THIS PERSON CAN BE CONTACTED AT ANY TIME, INCLUDING WEEKENDS, HOLIDAYS AND AFTER HOURS. THIS INFORMATION SHALL BE PROVIDED TO THE OWNER AND THE ENGINEER.

7. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL REQUIRED CONSTRUCTION PERMITS FROM ALL JURISDICTIONAL AUTHORITIES PRIOR TO START OF CONSTRUCTION. PERMIT COSTS ARE INCIDENTAL TO BASE BID.

8. ALL WORK ON THIS PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH APPLICABLE FEDERAL, STATE AND LOCAL LAWS, RULES AND REGULATIONS CONCERNING CONSTRUCTION SAFETY, HEALTH, AND ENVIRONMENTAL PROTECTION.

9. EXISTING SITE IMPROVEMENTS WHICH ARE DAMAGED OR DISPLACED BY THE CONTRACTOR SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE. REPAIRS SHALL BE APPROVED BY THE OWNER PRIOR TO CONSTRUCTION OF THE REPAIRS. REPAIRS SHALL BE ACCEPTED BY THE OWNER PRIOR TO FINAL PAYMENT.

10. THE CONTRACTOR SHALL USE THE DESIGNATED STAGING AREAS FOR STORAGE OF EQUIPMENT AND MATERIAL. NO MATERIAL OR EQUIPMENT MAY BE STORED OR LEFT ON-SITE AT ANY OTHER LOCATION. THE OWNER ASSUMES NO LIABILITY FOR CONTRACTOR'S EQUIPMENT AND MATERIAL IN THE STAGING AREA. SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. IF NO STAGING AREA IS DESIGNATED ON THESE PLANS, AN OFF-SITE STAGING AREA SHALL BE PROVIDED AT THE CONTRACTOR'S EXPENSE, OR THE CONTRACTOR MAY NEGOTIATE WITH THE OWNER TO USE AN ON-SITE AREA. OWNER WILL HAVE A STAGING AREA DESIGNATED.

11. ALL STATIONING REFERS TO THE CENTERLINE OF THE RIGHT-OF-WAY UNLESS OTHERWISE NOTED. STATIONING OF CHANNELS OR PIPES IN DRAINAGE EASEMENTS REFERS TO THE CENTERLINE OF CHANNEL OR PIPE, UNLESS OTHERWISE NOTED.

12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING, IN ADVANCE OF THEIR CONSTRUCTION OPERATIONS, IF OVERHEAD UTILITY LINES, SUPPORT STRUCTURES, POLES, GUYS, ETC., ARE AN OBSTRUCTION TO CONSTRUCTION OPERATIONS. IF ANY OBSTRUCTION IS EVIDENT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE APPROPRIATE UTILITY OWNER TO REMOVE OR SUPPORT THE UTILITY OBSTRUCTION. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.

13. FACILITIES WHICH ARE NOT SPECIFICALLY LOCATED WITH ACTUAL VERTICAL AND HORIZONTAL CONTROLS ON THE CONSTRUCTION DOCUMENTS, ARE SHOWN APPROXIMATE AND IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION PROVIDED BY VARIOUS OWNERS OF THE FACILITIES, AND SUPPLEMENTED BY VISUAL SURFACE INFORMATION WHERE APPROPRIATE. ACCURACY, LOCATION, AND COMPLETENESS OF THIS INFORMATION IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR AND SHOULD BE VERIFIED, BY ANY MEANS NECESSARY, BEFORE THE INITIATION OF CONSTRUCTION. SHOULD A CONFLICT EXIST, THE CONTRACTOR SHALL NOTIFY THE CITY, ENGINEER, AND THE CITY'S PROJECT MANAGER IMMEDIATELY.

14. IT IS MANDATORY THAT A PRECONSTRUCTION MEETING BE HELD BEFORE COMMENCING CONSTRUCTION. THE CONTRACTOR IS RESPONSIBLE FOR CONTACTING THE CITY'S PROJECT MANAGER TO DETERMINE THE TIME AND LOCATION OF THE PRECONSTRUCTION MEETING.

15. AS-BUILTS: CONTRACTOR SHALL DELIVER FINAL CERTIFIED AS-BUILTS IN HARD COPY. AS-BUILTS SHALL BE SUBMITTED WITH SUBSTANTIAL COMPLETION PAY APPLICATION. NO PAYMENT WILL BE MADE WITHOUT AS-BUILT SUBMITTAL.

16. CALL NM811 AT LEAST 2-DAYS BEFORE DIGGING.

17. THE CONTRACTOR SHALL CONFINE THEIR WORK TO WITHIN THE CONSTRUCTION LIMITS AND/OR PUBLIC RIGHT-OF-WAY TO PRESERVE EXISTING VEGETATION, LANDSCAPING, AND PRIVATE PROPERTY. APPROVAL OF THESE PLANS DOES NOT GIVE OR IMPLY ANY PERMISSION TO TRESPASS OR WORK ON PRIVATE PROPERTY. PERMISSION MUST BE GRANTED IN WRITING BY THE OWNER OF THAT PROPERTY.

EROSION CONTROL/ENVIRONMENTAL PROTECTION/STORM WATER POLLUTION PREVENTION PLAN

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FULFILLING ALL NECESSARY NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) REQUIREMENTS INCLUDING, BUT NOT LIMITED TO, OBTAINING AN NPDES PERMIT BEFORE CONSTRUCTION, FILLING OUT THE NOTICE OF INTENT (NOI) APPLICATION, AND FILLING OUT THE NOTICE OF TERMINATION (NOT) APPLICATION. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE IMPLEMENTATION OF AND INSPECTION REPORTS FOR THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP). THE CONTRACTOR SHALL SUBMIT THE SWPPP WITH THE PROPOSED CONSTRUCTION STAGING AREA AND TEMPORARY SANITARY FACILITIES CLEARLY SHOWN. ANY CHECK DAMS, SILT FENCES, OR OTHER BEST MANAGEMENT PRACTICES (BMP) THAT ARE REQUIRED IN THE APPROVED SWPPP SHALL BE INCLUDED IN AND ARE INCIDENTAL TO THE SWPPP BID AMOUNT.

2. THE CONTRACTOR IS REQUIRED TO KEEP A CURRENT COPY OF THE SWPPP AT THE CONSTRUCTION SITE OR AT AN EASILY ACCESSIBLE LOCATION SO THAT IT CAN BE MADE AVAILABLE AT THE TIME OF AN ONSITE INSPECTION OR UPON REQUEST BY THE EPA; A STATE, TRIBAL, OR LOCAL AGENCY APPROVING STORM WATER MANAGEMENT PLANS; THE OPERATOR OF A STORM SEWER SYSTEM RECEIVING DISCHARGES FROM THE SITE; OR REPRESENTATIVES OF THE U.S. FISH AND WILDLIFE SERVICE (USFWS) OR THE NATIONAL MARINE FISHERIES SERVICE (NMFS).

3. THE CONTRACTOR SHALL CONFORM TO ALL CITY, COUNTY, STATE AND FEDERAL DUST AND EROSION CONTROL REGULATIONS. THE CONTRACTOR SHALL PREPARE AND OBTAIN ANY NECESSARY DUST OR EROSION CONTROL PERMITS FROM THE REGULATORY AGENCIES.

4. THE CONTRACTOR SHALL EITHER PROMPTLY REMOVE ANY MATERIAL EXCAVATED WITHIN THE PUBLIC RIGHT-OF-WAY OR INSTALL BMPs ACCORDING TO NPDES REQUIREMENTS TO PREVENT DISCHARGE OF EXCAVATED MATERIAL WITHIN THE PUBLIC RIGHT-OF-WAY DURING A RAIN OR WIND EVENT. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.

5. THE CONTRACTOR SHALL IMPLEMENT THE APPROVED SWPPP AND ENSURE THAT NO SOIL ERODES FROM THE SITE INTO PUBLIC RIGHT-OF-WAY OR ONTO PRIVATE PROPERTY.

6. THE CONTRACTOR SHALL MITIGATE EROSION OF TEMPORARY OR PERMANENT DIRT SWALES BY INSTALLING BMPs IDENTIFIED IN THE APPROVED SWPPP IN THE SWALES PERPENDICULAR TO THE DIRECTION OF FLOW, AND AT INTERVALS AS SPECIFIED IN THE SWPPP.

7. CONSTRUCTION AREAS SHALL BE WATERED FOR DUST CONTROL IN COMPLIANCE WITH GOVERNMENT ORDINANCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND SUPPLYING WATER AS REQUIRED. WATERING, AS REQUIRED FOR CONSTRUCTION AND DUST CONTROL, SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION AND NO MEASUREMENT OR PAYMENT SHALL BE MADE THEREFORE.

8. ANY AREAS DISTURBED BY CONSTRUCTION AND NOT COVERED BY LANDSCAPING OR AN IMPERVIOUS SURFACE SHALL BE RE-VEGETATED WITH NATIVE GRASS SEEDING. WHEN CONSTRUCTION ACTIVITIES CEASE AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME WITHIN 14 DAYS, STABILIZATION MEASURES MUST BE INITIATED. UNLESS INDICATED OTHERWISE ON THESE PLANS OR ON THE LANDSCAPING PLAN, NATIVE GRASS SEEDING SHALL BE IN ACCORDANCE WITH SECTION 1012 OF THE NEW MEXICO STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION, APWA NM CHAPTER, CURRENT EDITION.

9. ALL WASTE PRODUCTS FROM THE CONSTRUCTION SITE, INCLUDING ITEMS DESIGNATED FOR REMOVAL, CONSTRUCTION WASTE, CONSTRUCTION EQUIPMENT WASTE PRODUCTS (OIL, GAS, TIRES, ETC.) GARBAGE, GRUBBING, EXCESS CUT MATERIAL, VEGETATIVE DEBRIS, ETC. SHALL BE APPROPRIATELY DISPOSED OF OFFSITE AT NO ADDITIONAL COST TO THE CITY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN PERMITS REQUIRED TO HAUL OR DISPOSE OF WASTE PRODUCTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE WASTE DISPOSAL SITE COMPLIES WITH GOVERNMENT REGULATIONS REGARDING THE ENVIRONMENT, ENDANGERED SPECIES, AND ARCHAEOLOGICAL RESOURCES.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CLEANUP AND REPORTING OF SPILLS OF HAZARDOUS MATERIALS ASSOCIATED WITH THE CONSTRUCTION SITE. HAZARDOUS MATERIALS INCLUDE GASOLINE, DIESEL FUEL, MOTOR OIL, SOLVENTS, CHEMICALS, PAINTS, ETC. WHICH MAY BE A THREAT TO THE ENVIRONMENT. THE CONTRACTOR SHALL REPORT THE DISCOVERY OF PAST OR PRESENT SPILLS TO THE NEW MEXICO ENVIRONMENT DEPARTMENT EMERGENCY RESPONSE TEAM AT (505) 827-9329.

11. THE CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE REGULATIONS CONCERNING SURFACE AND UNDERGROUND WATER. CONTACT WITH SURFACE WATER BY CONSTRUCTION EQUIPMENT AND PERSONNEL SHALL BE MINIMIZED. EQUIPMENT MAINTENANCE AND REFUELING OPERATIONS SHALL BE PERFORMED IN AN ENVIRONMENTALLY SAFE MANNER IN COMPLIANCE WITH GOVERNMENT REGULATIONS.

12. WHERE STORM INLETS ARE SUSCEPTIBLE TO INFLOW OF SILT OR DEBRIS FROM CONSTRUCTION ACTIVITIES, PROTECTION SHALL BE PROVIDED ON THEIR UPSTREAM SIDE UTILIZING BMPs ACCORDING TO NPDES REQUIREMENTS. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.

13. STORM WATER POLLUTION PREVENTION PLANS (SWPPP) AND ACCOMPANYING FEDERAL EPA ADMINISTRATIVE PROCEDURES SHALL MEET THE CITY OF HOBBS GUIDELINES AND PROCEDURES OUTLINED IN THE CURRENT ADDITION OF THE NEW MEXICO STATE HIGHWAY AND TRANSPORTATION DEPARTMENT STORM WATER MANAGEMENT GUIDELINES FOR CONSTRUCTION AND INDUSTRIAL ACTIVITIES MANUAL.

14. THE CONTRACTOR SHALL PROVIDE ADEQUATE MEANS FOR CLEANING TRUCKS AND/OR OTHER EQUIPMENT OF MUD BEFORE ENTERING PUBLIC STREETS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CLEAN STREETS AND TAKE WHATEVER MEASURES ARE NECESSARY TO ENSURE THAT ALL ROADS ARE MAINTAINED IN A CLEAN, MUD AND DUST-FREE CONDITION AT ALL TIMES.

ROADWAY GENERAL NOTES

1. NO PAVING CONSTRUCTION ACTIVITIES SHALL BE STARTED UNTIL ALL UNDERGROUND UTILITIES WITHIN THE ROADWAY ARE COMPLETED, TESTED, AND APPROVED. ALL WATER VALVE BOXES AND ELECTRICAL, TELEPHONE, TELEVISION, AND SEWER MANHOLES IN THE CONSTRUCTION AREA SHALL BE ADJUSTED TO FINISHED GRADE.

2. ALL SIGNS, BARRICADES, CHANNELIZATION DEVICES, PAVEMENT MARKINGS, SIGN FRAMES AND ERECTION OF SUCH DEVICES SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (MUTCD), CURRENT EDITION.

3. ALL STREET STRIPING ALTERED OR DESTROYED DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR TO MATCH THE ORIGINAL CONDITIONS (I.E. TYPE, SPACING) AT THE LOCATION PRIOR TO CONSTRUCTION, OR AS SHOWN IN THIS PLAN SET.

4. STREET GRADES SHALL BE RESTORED BY THE CONTRACTOR TO THE EXISTING GRADES UNLESS OTHERWISE DIRECTED BY THE CITY OF HOBBS. SMOOTH TRANSITIONS SHALL BE MADE BETWEEN EXISTING PAVEMENT WHICH REMAINS IN PLACE AND PAVEMENT WHICH IS REPLACED. WHEN ABUTTING NEW PAVEMENT TO EXISTING, SAW CUT BACK EXISTING PAVEMENT TO A NEAT, STRAIGHT LINE AS REQUIRED TO REMOVE ANY BROKEN OR CRACKED PAVEMENT.

5. THE LOCATION OF ALL VALVES AND MANHOLES MUST BE REFERENCED AT ALL TIMES BY THE CONTRACTOR DURING CONSTRUCTION AND MADE ACCESSIBLE DAILY UPON COMPLETION OF ALL PAVING ACTIVITIES.

UTILITIES-GENERAL NOTE:

1. IF ANY UTILITY LINES, PIPELINES OR UNDERGROUND UTILITY LINES ARE SHOWN ON THESE DRAWINGS. THEY ARE SHOWN IN AN APPROXIMATE LOCATION ONLY, AND LINES MAY EXIST WHERE NONE ARE SHOWN. THE LOCATION IS BASED UPON INFORMATION PROVIDED BY THE UTILITY OWNER OR FROM EXISTING PLANS, AND THIS INFORMATION MAY BE INCOMPLETE, OR OBSOLETE AT THE TIME OF CONSTRUCTION. THE ENGINEER HAS NOT UNDERTAKEN ANY FIELD VERIFICATION OF THESE LOCATIONS, LINE SIZES OR MATERIAL TYPE, MAKES NO REPRESENTATION THERETO, AND ASSUMES NO RESPONSIBILITY ITSELF OF THE LOCATION OF ANY UTILITY LINE, PIPELINE OR UNDERGROUND INSTALLATION IN OR NEAR THE AREA IN ADVANCE OF ANY DURING ANY EXCAVATION WORK. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES CAUSED BY ITS FAILURE TO LOCATE, IDENTIFY AND PRESERVE ANY AND ALL EXISTING UTILITIES, PIPELINES AND UNDERGROUND FACILITIES. IN PLANNING AND CONDUCTING EXCAVATIONS, THE CONTRACTOR SHALL COMPLY WITH ALL STATE STATUES, MUNICIPAL AND LOCAL ORDINANCES, RULES AND REGULATIONS, IF ANY, PERTAINING TO THE LOCATION OF THESE LINES AND FACILITIES

2. THE CONTRACTOR SHALL NOTIFY NEW MEXICO ONE CALL, INC. AT PHONE NO. (811) AT LEAST TWO WORKING DAYS PRIOR TO STARTING WORK ON THIS PROJECT.

3. SEWER/WATER LINES SHALL BE PLACED IN SEPARATE TRENCHES A DISTANCE OF 15 FEET TYPICALLY OR A MINIMUM OF 10 FEET APART HORIZONTALLY. THE WATER LINE SHALL BE PLACED A MINIMUM OF 1.5 FEET HIGHER IN ELEVATION THAN THE SEWER. AT ALL CROSSINGS OF WATER AND SEWER LINES, THE WATER LINE SHALL BE MINIMUM OF 1.5 HIGHER THAN THE SEWER OR THE SEWER LINE SHALL BE SDR 26 PRESSURIZED PIPE. IF THESE REQUIREMENTS ARE NOT MET, CONSTRUCT SEWER ENCASEMENT PER APWA STD. DWG. 2140.

4. SEWER & WATER LINE DISTANCES SHOWN IN PLANS ARE HORIZONTAL DISTANCES WITHOUT REGARD TO SLOPE OF PIPE OR PROJECT STATIONING.

5. ELECTRONIC MARKER DEVICES (EMD) WILL BE REQUIRED AT ALL SANITARY SEWER MANHOLES AND WATER VALVE CANS, NEW OR ADJUSTED TO GRADE THROUGHOUT THE PROJECT. EMD's SHALL BE INCIDENTAL TO ITEM OR ADJUSTMENT.

SOILS

1. UNLESS OTHERWISE SPECIFIED SUBGRADE SOILS AND STRUCTURAL FILL MATERIALS SHALL BE COMPACTED TO THE FOLLOWING PERCENTAGES OF THE ASTM D-1557 MAXIMUM DENSITY.

MATERIALS	PERCENT (%) COMPACTION
STRUCTURAL FILL IN THE BUILDING AREA	95
SUB BASE FOR SLAB SUPPORT	95
MISCELLANEOUS BACKFILL BELOW STRUCTURAL FILL OR ROAD	95
MISCELLANEOUS BACKFILL BELOW UNPAVED, NON-BUILDING AREAS	90
ROAD SUB GRADE	95
SIDEWALK SUB GRADE	95
CURB AND GUTTER SUBGRADE	95
ARROYOS	90

DESIGNED: RJF

DRAWN: RJS

CHECKED: RJF

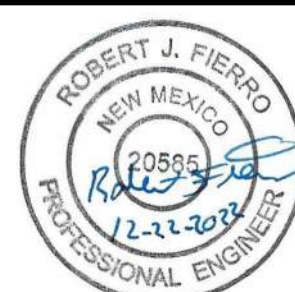
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DATE

BY

REVISION





**Fierro & Company**  
ENGINEERING | SURVEYING  
3201 4th STREET NW, SUITE C  
ALBUQUERQUE, NM 87107  
(505) 352-8930

CLIENT:

**JOHN LEMKE  
D.B.A.  
LEMKE DEVELOPMENT, INC.**

**MEADOWLANDS  
UNIT II**

**GENERAL NOTES**

SHEET 2 of 31

SET 1-2

DATE Dec-2022

PROJ 21068

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## WASTEWATER GENERAL NOTES

1. SEWER/WATER LINES SHALL BE PLACED IN SEPARATE TRENCHES AT A DISTANCE OF 15 FEET TYPICALLY OR A MINIMUM OF 10 FEET APART HORIZONTALLY. THE WATER LINE SHALL BE PLACED A MINIMUM OF 1.5 FEET HIGHER IN ELEVATION THAN THE SEWER LINE. AT ALL CROSSINGS OF WATER AND SEWER LINES, THE WATER LINE SHALL BE A MINIMUM OF 1.5 FEET HIGHER THAN THE SEWER LINE OR THE SEWER LINE SHALL BE C-900 PRESSURIZED PIPE.
2. IT WILL BE THE CONTRACTOR'S SOLE RESPONSIBILITY TO PROTECT AND MAINTAIN IN SERVICE ALL EXISTING UTILITIES. THE CONTRACTOR SHALL ADEQUATELY SUPPORT AND PROTECT EXISTING UTILITIES AFFECTED BY THE CONTRACTOR'S TRENCHING ACTIVITY. IN THE EVENT THAT EXISTING UTILITIES ARE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL ARRANGE FOR AND COORDINATE WITH THE PROJECT MANAGER, PROMPT REPAIR BY THE RESPECTIVE UTILITY AND SHALL BEAR THE COST OF THE REPAIRS.
3. THE CITY OF HOBBS SHALL APPROVE MATERIAL SUBMITTALS BEFORE CONSTRUCTION
4. PRIOR TO THE SEWER LINE INSTALLATION, THE FOLLOWING CONDITIONS WILL OCCUR:
  - A) THE SEWER LINE ROUTE WILL BE CLEARED AND GRUBBED AND THEN GRADED TO PLAN ELEVATION
  - B) THE SEWER LINE WILL BE STAKED WHEN OUTSIDE AN AREA WITH CURB AND GUTTER
5. 30 DAYS FOLLOWING INSTALLATION AND BACKFILL OF SEWER LINES, A DEFLECTION TEST USING A HAND PULLED MANDREL SHALL BE PERFORMED IN THE PRESENCE OF THE CITY'S INSPECTOR. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.
6. AIR TESTING OF SEWER LINES AND HYDROSTATIC TESTING OF FORCE MAINS SHALL BE CONDUCTED IN THE PRESENCE OF THE CITY'S INSPECTOR. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.
7. ALL SEWER SERVICE LINES SHALL BE INSPECTED BY TV CAMERA AND VIDEOED THEN PROVIDED TO THE CITY'S INSPECTOR FOR REVIEW BEFORE ACCEPTANCE BY THE CITY. IN THE EVENT THAT THE FIRST INSPECTION OR SUBSEQUENT INSPECTIONS AFTER THAT DO NOT PASS, THE CONTRACTOR WILL BE REQUIRED TO PERFORM ADDITIONAL INSPECTIONS OF THE SEWER SERVICE LINES USING A TV CAMERA AT THE CONTRACTOR'S EXPENSE.
8. MANHOLES SHALL MEET THE CITY OF HOBBS STANDARDS EXCEPT THAT THERE SHALL BE NO LADDER RUNGS INSTALLED.
9. THE CONTRACTOR IS RESPONSIBLE FOR TESTING OF ALL FORCE MAIN LINES, INCLUDING BUT NOT LIMITED TO HYDROSTATIC AND BACTERIA TESTING, DISINFECTING, AND FLUSHING. ALL COSTS FOR THESE REQUIREMENTS ARE INCIDENTAL TO THE CONTRACT.
10. IF BYPASS PUMPING IS REQUIRED, THEN A BYPASS PUMPING PLAN MUST BE SUBMITTED TO THE CITY'S PROJECT MANAGER, FOR ACCEPTANCE, SEVEN (7) DAYS BEFORE BYPASS PUMPING BEGINS.
11. MANHOLES SHALL BE RAISED TO SURFACE COURSE OF PAVEMENT. CIRCULAR CONCRETE COLLARS SHALL BE CONSTRUCTED TO SURFACE ELEVATION.
12. NO BRICKS SHALL BE USED TO ADJUST MANHOLES TO FINISHED GRADE.

	EXISTING MAJOR CONTOUR
	EXISTING MINOR CONTOUR
	PROPOSED MAJOR CONTOUR
	PROPOSED MINOR CONTOUR
	PROPERTY BOUNDARY
	EXISTING MAIN WATERLINE
	EXISTING SANITARY SEWER
	NEW MAIN WATERLINE
	NEW SANITARY SEWER
	NEW WATER SERVICE
	NEW SANITARY SERVICE
	EXISTING STREET LIGHT
	PROPOSED STREET LIGHT
	EXISTING SEWER MANHOLE
	NEW SEWER MANHOLE
	EXISTING FIREHYDRANT
	PROPOSED FIRE HYDRANT
	EXISTING WATER VALVE
	NEW WATER VALVE
TC	TOP OF CURB
TA	TOP OF ASPHALT
	HIGH POINT
	EXISTING PAVEMENT
	EXISTING CONCRETE
	NEW PAVEMENT
	NEW CONCRETE

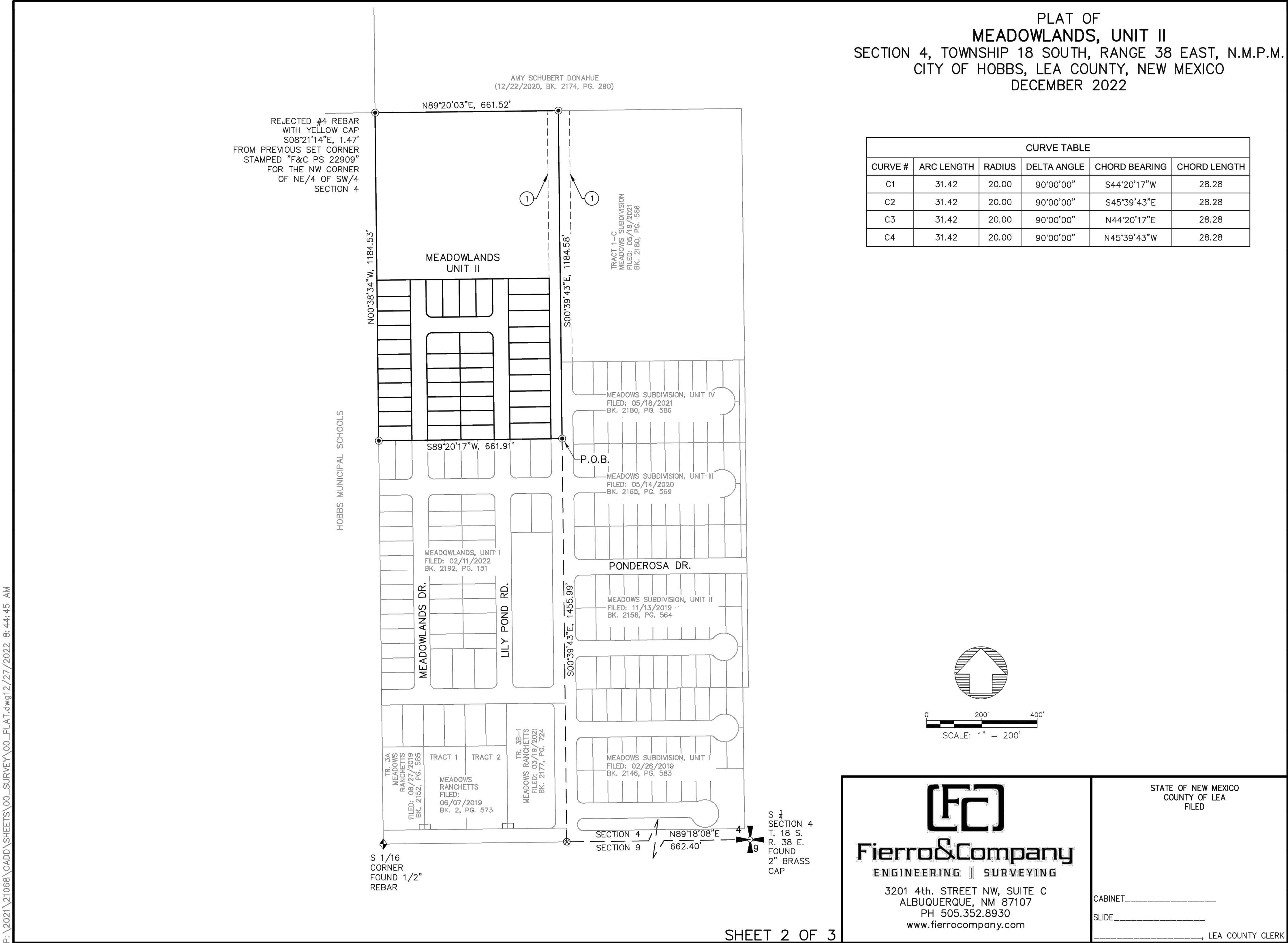
DESIGNED: RJF											<div> <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930</div>	CLIENT:  <div>JOHN LEMKE D.B.A. LEMKE DEVELOPMENT, INC.</div>	MEADOWLANDS UNIT II	SHEET 3 of 31
DRAWN: RJS								SET 1-3						
CHECKED: RJF									GENERAL NOTES				DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY		REVISION								PROJ 21068	





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	NO.	DATE	BY		REVISION		



**Fierro & Company**  
ENGINEERING | SURVEYING  
3201 4th. STREET NW, SUITE C  
Albuquerque, NM 87107  
(505) 352-8930

CLIENT:  
**JOHN LEMKE**  
**D.B.A.**  
**LEMKE DEVELOPMENT, INC.**

**MEADOWLANDS  
UNIT II**

**PRELIMINARY PLAT**

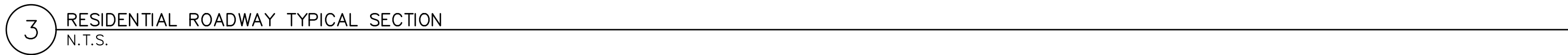
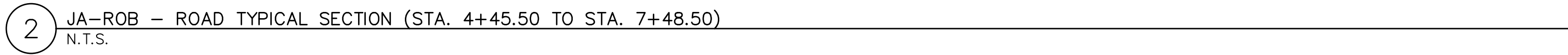
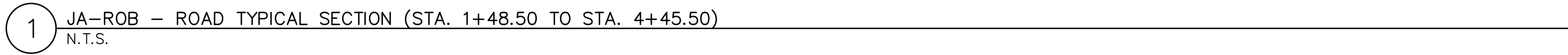
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## HOBBS MUNICIPAL SCHOOLS



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DRAWN: RJS						SET 3-1						
CHECKED: RJF						DATE Dec-2022						
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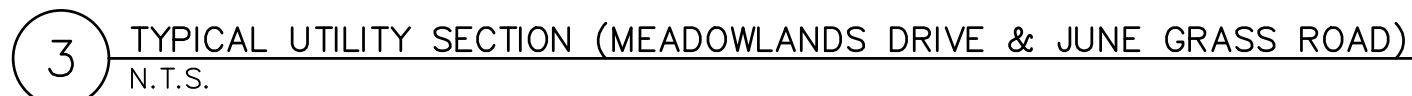
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DRAWN: RJS							SET 3-4						
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1. DITCHES, BACKFILL MATERIALS, BACKFILL & COMPACTION PROVIDED BY DITCHING CONTRACTOR
2. DITCHING CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH ALL UTILITY OWNERS AND FOR THE PROTECTION OF ALL FACILITIES AFTER THEIR INSTALLATION. IF APPLICABLE, GAS COMPANY SHALL DITCH, INSTALL, BACKFILL AND COMPACT THEIR OWN UTILITIES.
3. ANY UTILITIES DAMAGED BY THE DITCHING CONTRACTOR DURING TRENCHING, COMPACTION, OR INSTALLING SERVICE LATERALS SHALL REPAIRED AND/OR REPLACED AT THE EXPENSE OF THE DITCHING CONTRACTOR.
4. EXTRA DEPTH NECESSARY FOR THE ELECTRICAL PRIMARY AND SECONDARY TO CROSS UNDER WATER OR SEWER LINES AS SHOWN IN THE TYPICAL DETAILS IN CONSIDERED INCIDENTAL AND NO EXTRA PAYMENT WILL BE MADE.
5. ELECTRICAL CONDUIT REQUIRES A MINIMUM RADIUS OF 48 INCHES FOR ANY BRANCHES / BENDS. CONTRACTOR SHALL PROVIDE ADEQUATE DITCH WIDTH TO ACCOMMODATE THE NECESSARY MINIMUM RADIUS OF 48 INCHES REQUIRED AT ALL BRANCHES / BENDS FOR ELECTRICAL CONDUIT. THIS WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL PAYMENT WILL BE MADE.
6. JAMES JAROB IS XCEL'S REPRESENTATIVE ON THE MEADOWS SUBDIVISION. EMAIL: JAMES.M.JAROB@XCELENERGY.COM FOR ANY QUESTION REGARDING LAYOUT. SHEET NO. 11 IS DESIGNATED FOR XCEL'S ELECTRICAL PLAN.



**PLAN VIEW**

**SECTION A-A**

**SECTION B-B**

**KEYED NOTES:**

- 30" FRAME & COVER. REFER TO DESIGN STANDARD NO. 209.00
- 4000 PSI CONCRETE COLLAR, REFER TO DESIGN STANDARD NO. 205.00 FOR PAVED AREAS (NON PAVED AREAS FOLLOW THIS DETAIL)
- ADJUSTMENT RINGS (MAX. HEIGHT OF 18")
- MANHOLE SHALL CONFORM TO ASTM D-3753 (LATEST EDITION) AND CERTIFIED TO BE H-20 LOAD RATED
- PIPE CUTOUT IN MANHOLE BARREL, MAX. OF 1" CLEARANCE BETWEEN MANHOLE AND O.D. OF PIPE

**INSTALLATION NOTES:**

- LEVEL SUB GRADE MATERIAL. ENSURE FOUNDATION MEETS COMPACTION REQUIREMENTS, AND PLACE 4" EA 0x8"x10" HOLLOW CINDER BLOCKS AS SHOWN.
- MAKE CUTOUT FOR PIPE PENETRATION TO FOLLOW CURVATURE OF THE PIPE AND WITH A MAXIMUM OF 1" CLEARANCE. SEAL CUT EDGES WITH RESIN. PIPE PENETRATION CUTOUT MAY EXTEND TO THE BARREL BOTTOM (AS SHOWN IN SECTION B). PLACE MANHOLE LEVEL, ON BLOCKS.
- FORM SURFACE AND MOUND CONCRETE AROUND PIPE PENETRATIONS, TO FORM A SEAL, IN ONE CONTINUOUS PLACEMENT OPERATION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING THE FIBERGLASS MANHOLE AND CONE SECTION DURING ALL CONSTRUCTION OPERATIONS INCLUDING PAVING AND MANHOLE ADJUSTMENT. THE CONTRACTOR SHALL SCHEDULE FOR INSPECTION PRIOR TO PLACING CONCRETE BASE, BACKFILL, AND MANHOLE ADJUSTMENT.
- REBAR IN BASE SHALL EXTEND 10" MINIMUM BEYOND MANHOLE WALL

**TYPE "C" FIBERGLASS MANHOLE**

**CITY OF HOBBS ENGINEERING DEPARTMENT  
CONSTRUCTION DETAILS & SPECIFICATIONS**

**DESIGN STD. NO.: 21150-12-2016**

**SCALE: NTS**

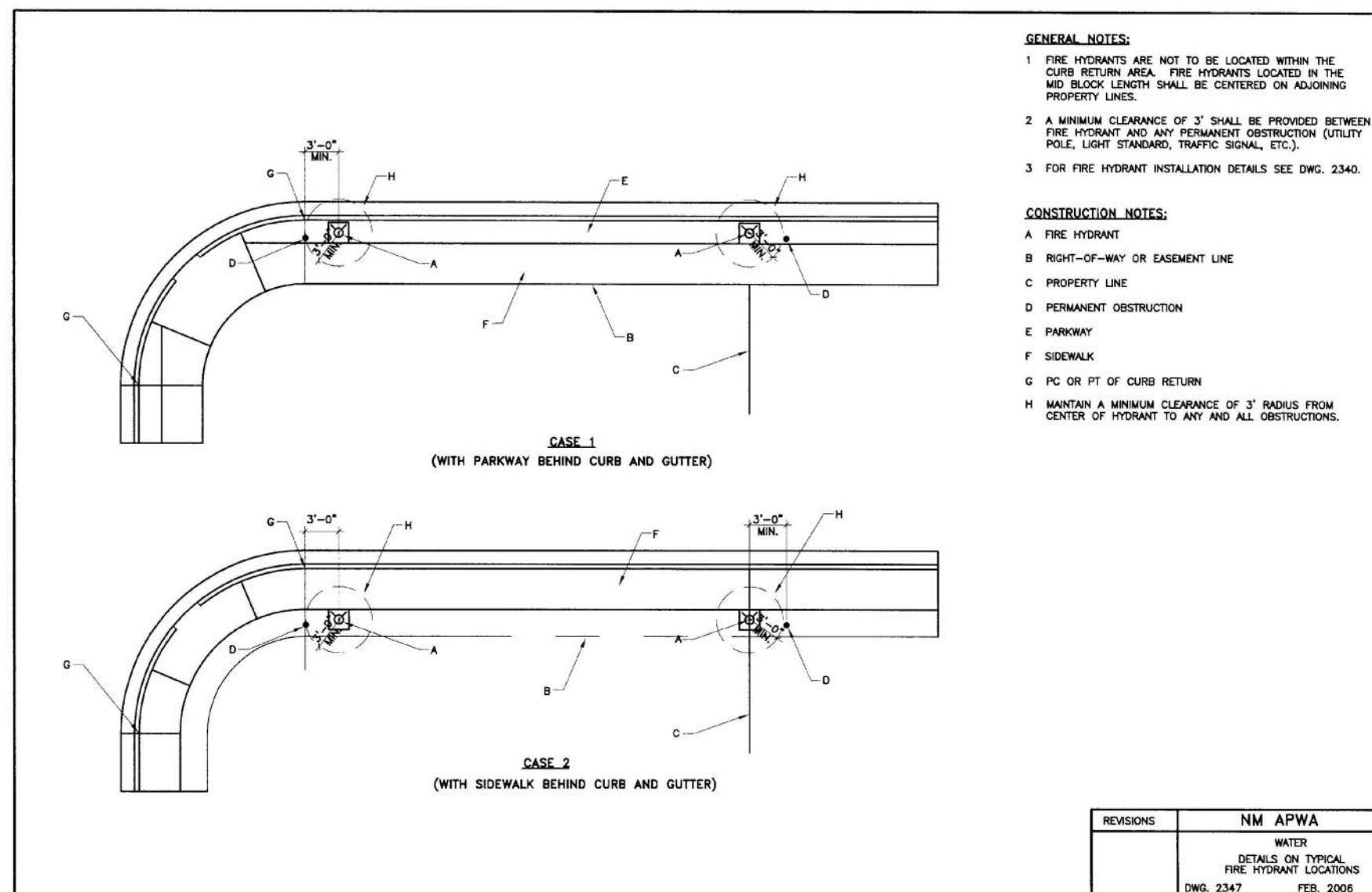
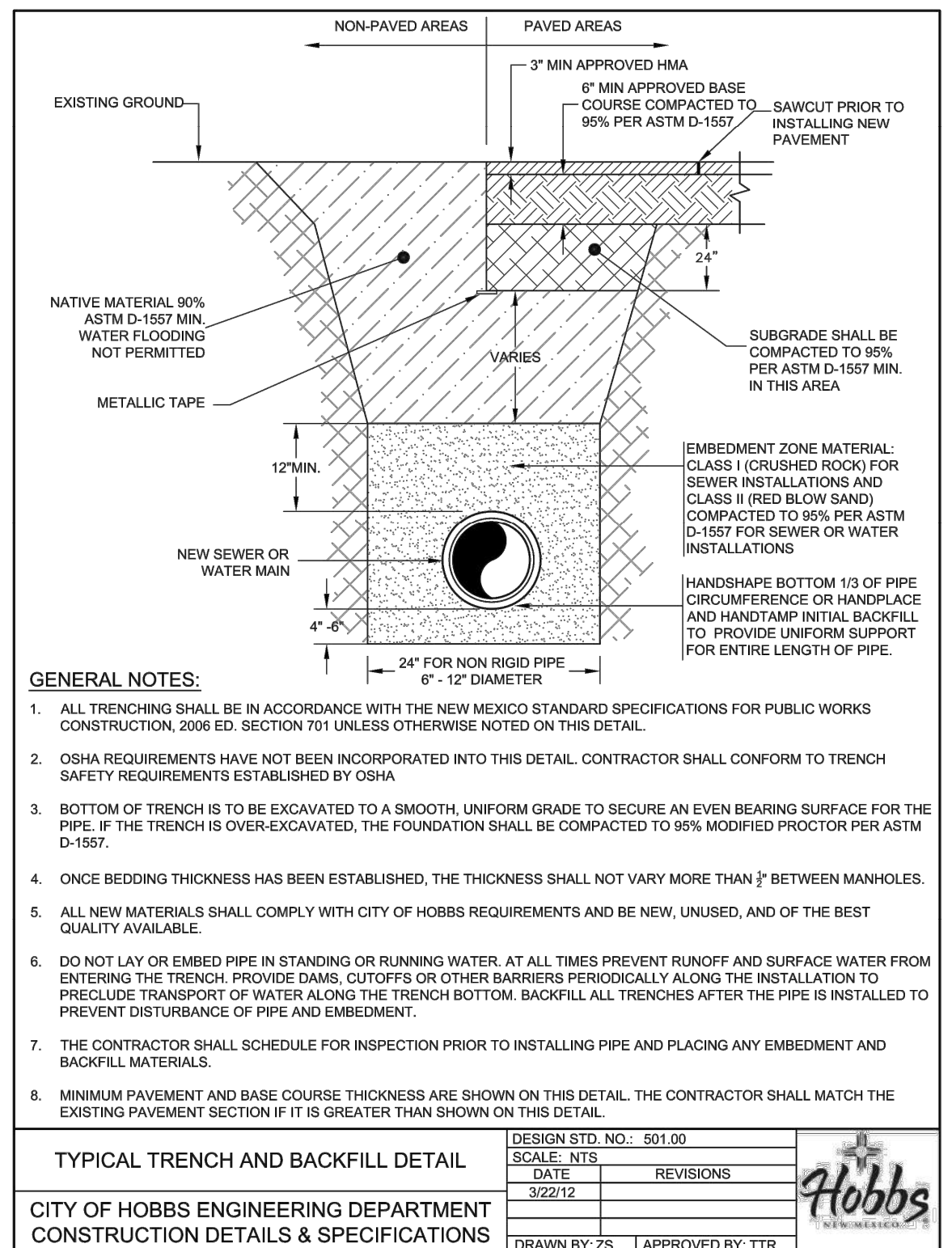
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**REVISIONS**

**APPROVED BY: TTR**

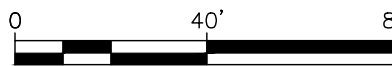
**Hobbs**  
NEW MEXICO





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DRAWN: RJS					SET 3-8					
CHECKED: RJF									<b>UTILITY DETAILS</b>	DATE Dec-2022
APPROVED: ###	NO.	DATE	BY	REVISION						PROJ 21068

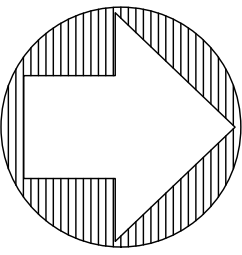




SHEET	15	of	31
SET	4-0		
DATE	Dec-2022		
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- 1 EXISTING ASPHALT PAVEMENT TO REMAIN
- 2 EXISTING CURB AND GUTTER TO REMAIN.
- 3 EXISTING SIDEWALK TO REMAIN.
- 4 CONSTRUCT ASPHALT PAVING SECTION  
(MINOR COLLECTOR) PER DETAIL 2/3-2
- 5 CONSTRUCT ASPHALT PAVING SECTION  
(RESIDENTIAL) PER DETAIL 3/3-2
- 6 CONSTRUCT 6" x 24" (STANDARD) CURB & GUTTER  
PER C.O.H. DESIGN STD. NO. 310.01
- 7 CONSTRUCT LAYDOWN CURB 30"  
PER C.O.H. DESIGN STD. NO. 314.30
- 8 CONSTRUCT TYPICAL SIDEWALK  
PER C.O.H. DESIGN STD NO. 300.00
- 9 CONSTRUCT CONCRETE VALLEY GUTTER  
PER DETAIL 4/3-2
- 10 CONSTRUCT ADA RAMP - CURB RETURN  
PER C.O.H. DESIGN STD. NO. 305
- 11 CONSTRUCT CUT-OFF WALL PER DETAIL 1/3-2
- 12 INSTALL STOP SIGN AND STREET NAME SIGN  
PER MUTPC, AT OWNERS' DIRECTION



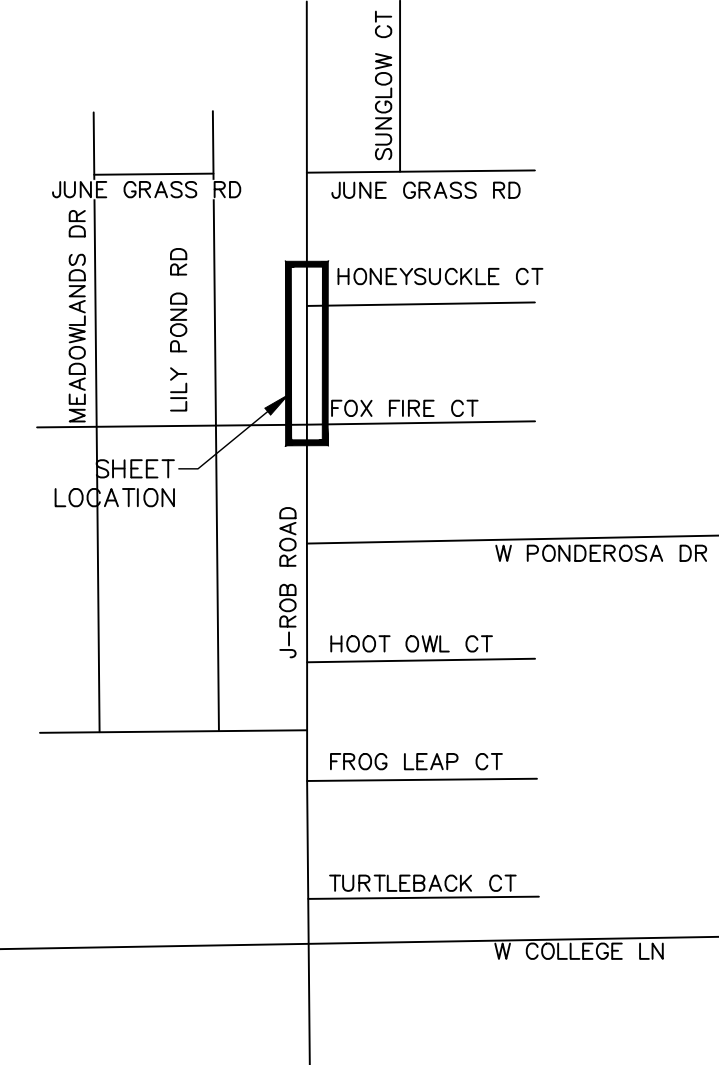
0 20' 40'

HORIZONTAL

0 4' 8'

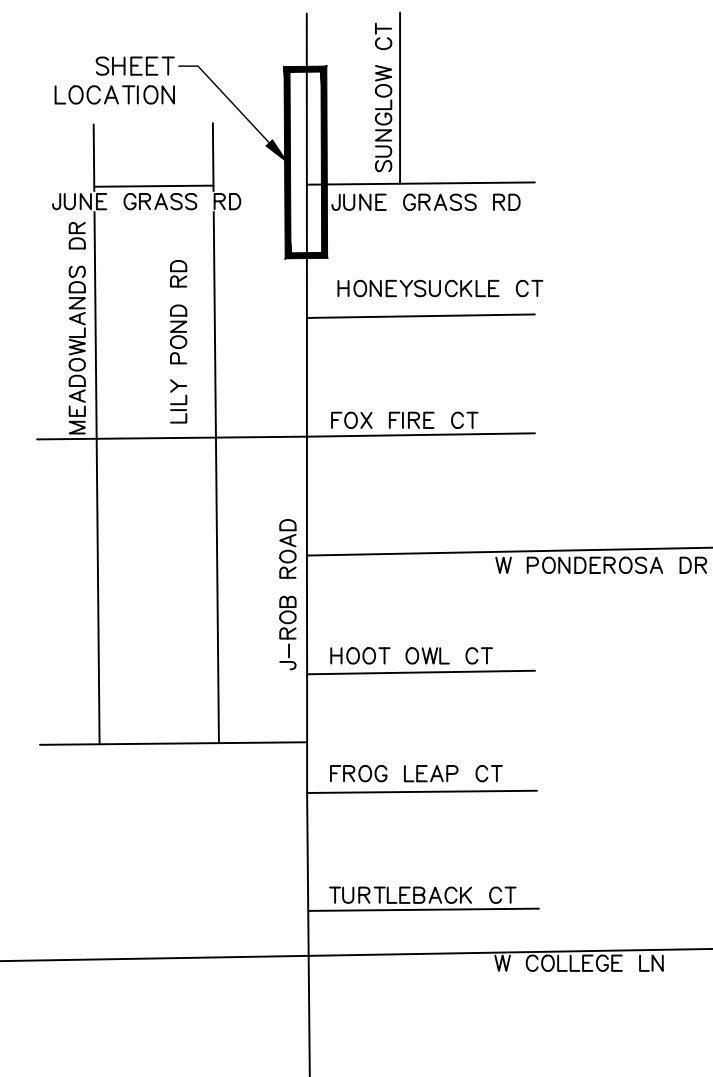
VERTICAL

## KEY MAP

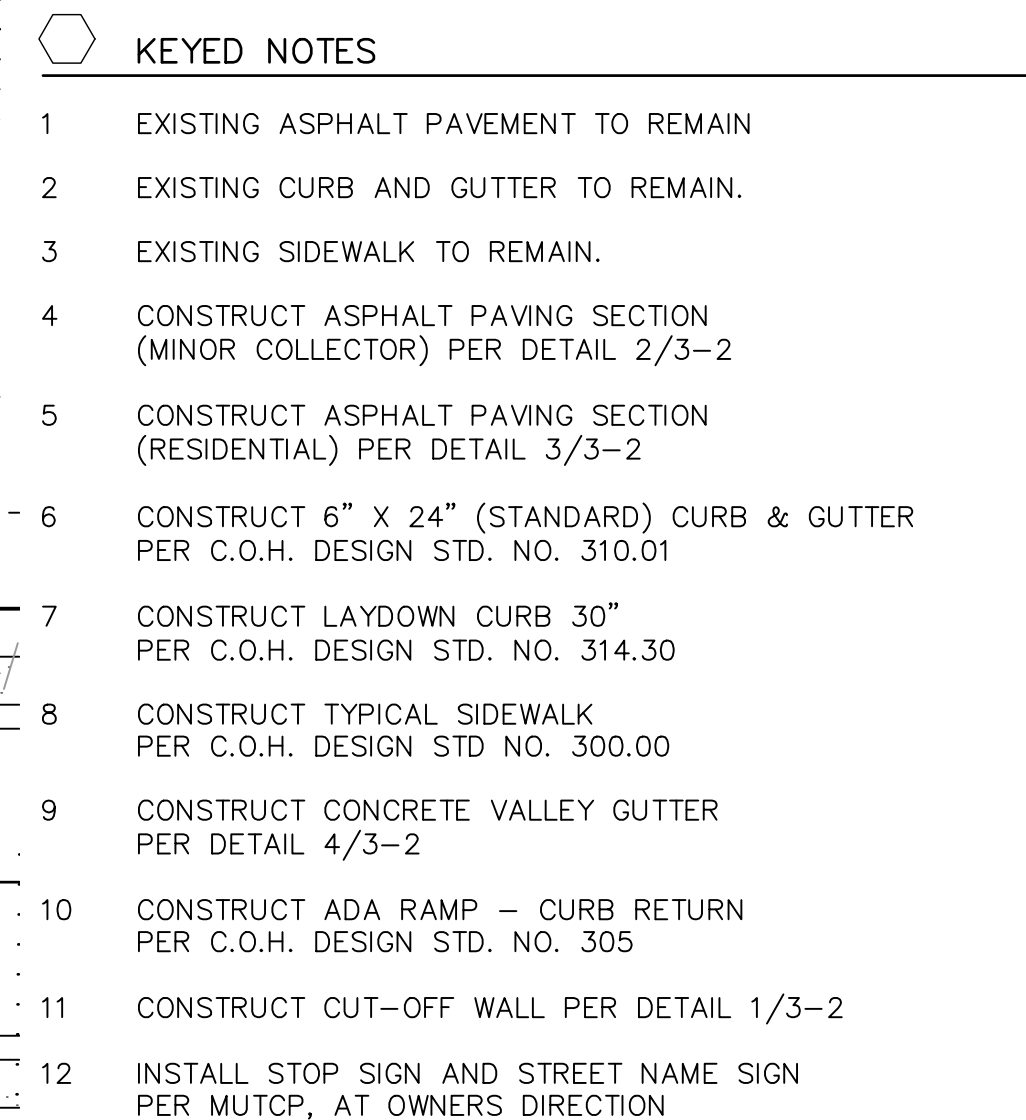
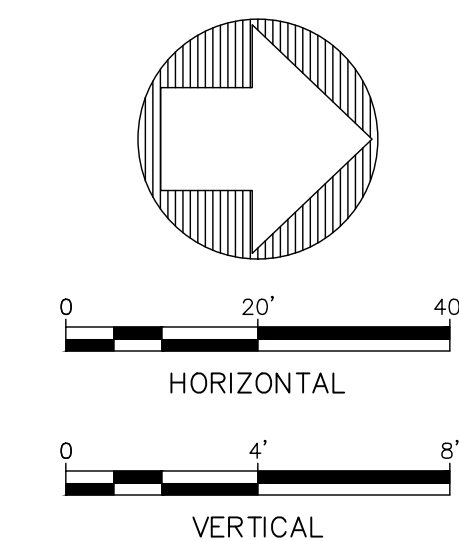


DESIGNED: RJF							 <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930	CLIENT:  <b>JOHN LEMKE</b> <b>D.B.A.</b> <b>LEMKE DEVELOPMENTS, INC.</b>	<b>MEADOWLANDS</b> <b>UNIT II</b>		SHEET 16 of 31	
DRAWN: RJS											SET 4-1	
CHECKED: RJF										<b>JA-ROB RD</b> <b>ROADWAY PLAN &amp; PROFILE</b>		DATE Dec-2022
APPROVED: ###	NO.	DATE	BY	REVISION								PROJ 21068

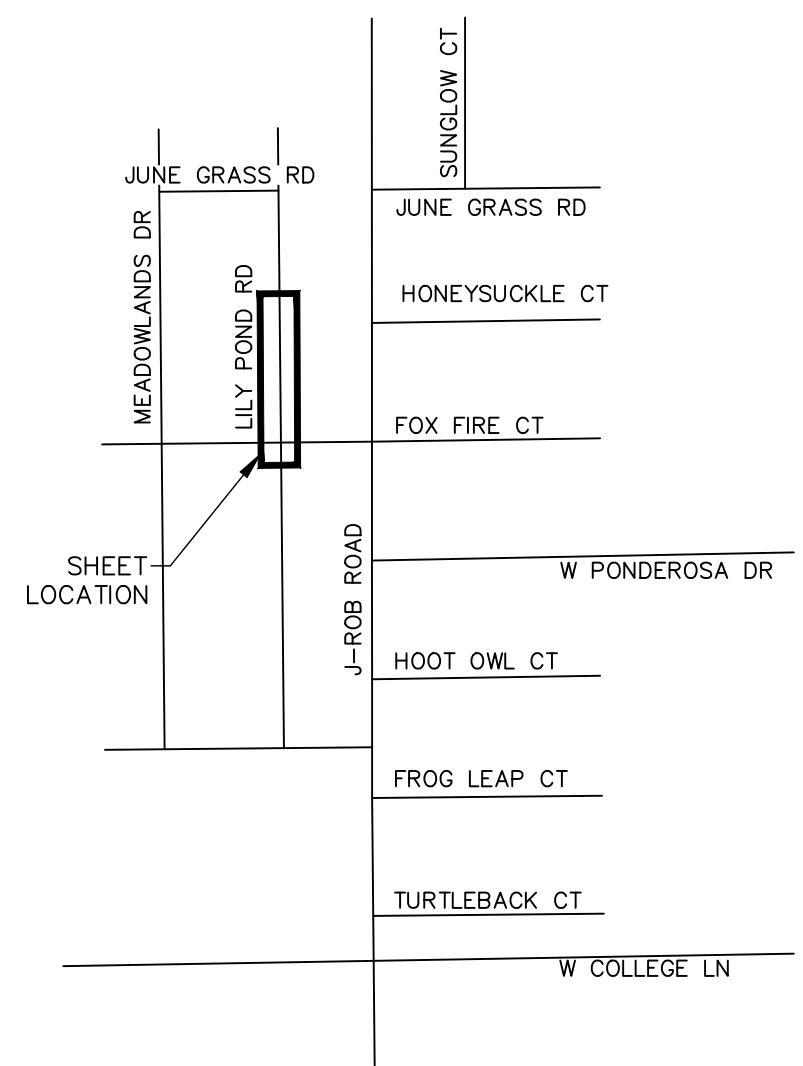






SHEET	17	of	31
SET	4-2		
DATE	Dec-2022		
PROJ	21068		

[illegible]

## KEY MAP



DESIGNED: RJF								<div> <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930</div>	CLIENT: <div><b>JOHN LEMKE</b> <b>D.B.A.</b> <b>LEMKE DEVELOPMENT, INC.</b></div>	<b>MEADOWLANDS</b> <b>UNIT II</b>	SHEET 18 of 31
DRAWN: RJS						SET 4-3					
CHECKED: RJF						<b>LILY POND RD</b> <b>ROADWAY PLAN &amp; PROFILE</b>				DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION						PROJ 21068	





SHEET LOCATION

MEADOWS DR

JUNIPER GRASS RD

LILY POND RD

HONEY SUCKLE CT

FOX FIRE CT

J-ROB ROAD

W PONDEROSA DR

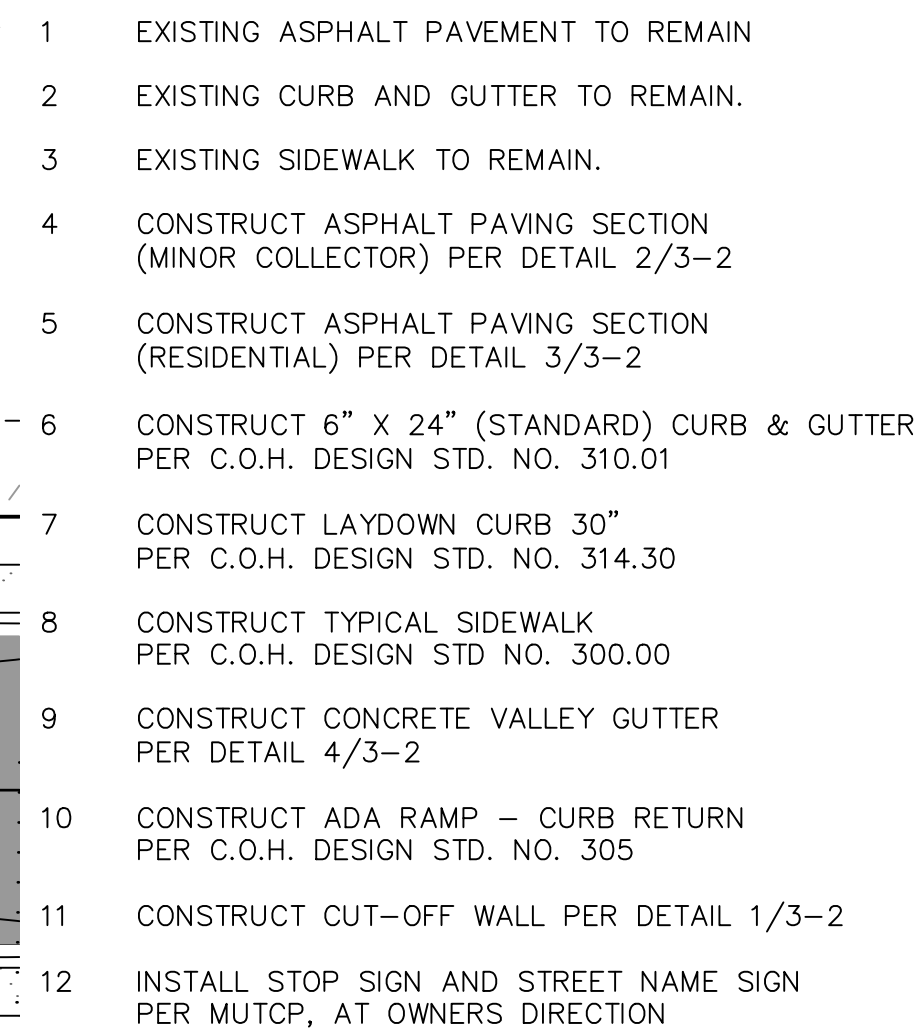
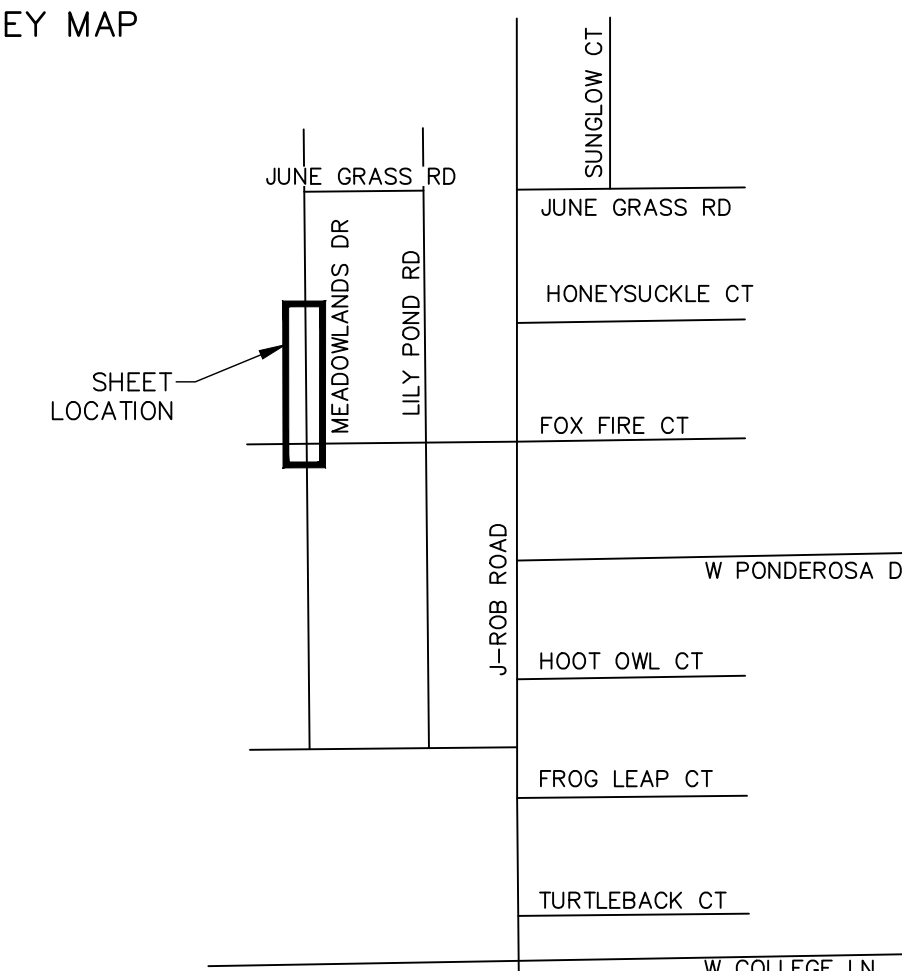
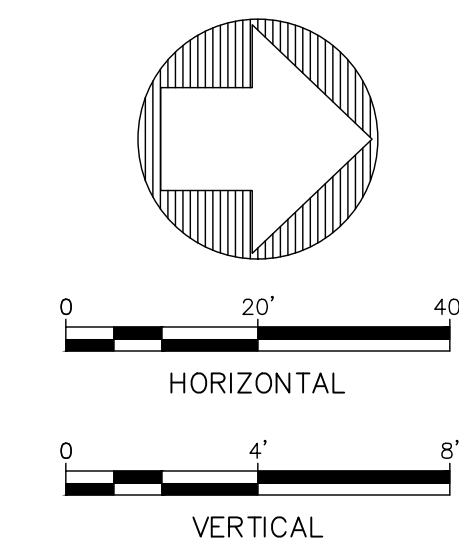
HOOT OWL CT



FROG LEAP CT

TURTLEBACK CT

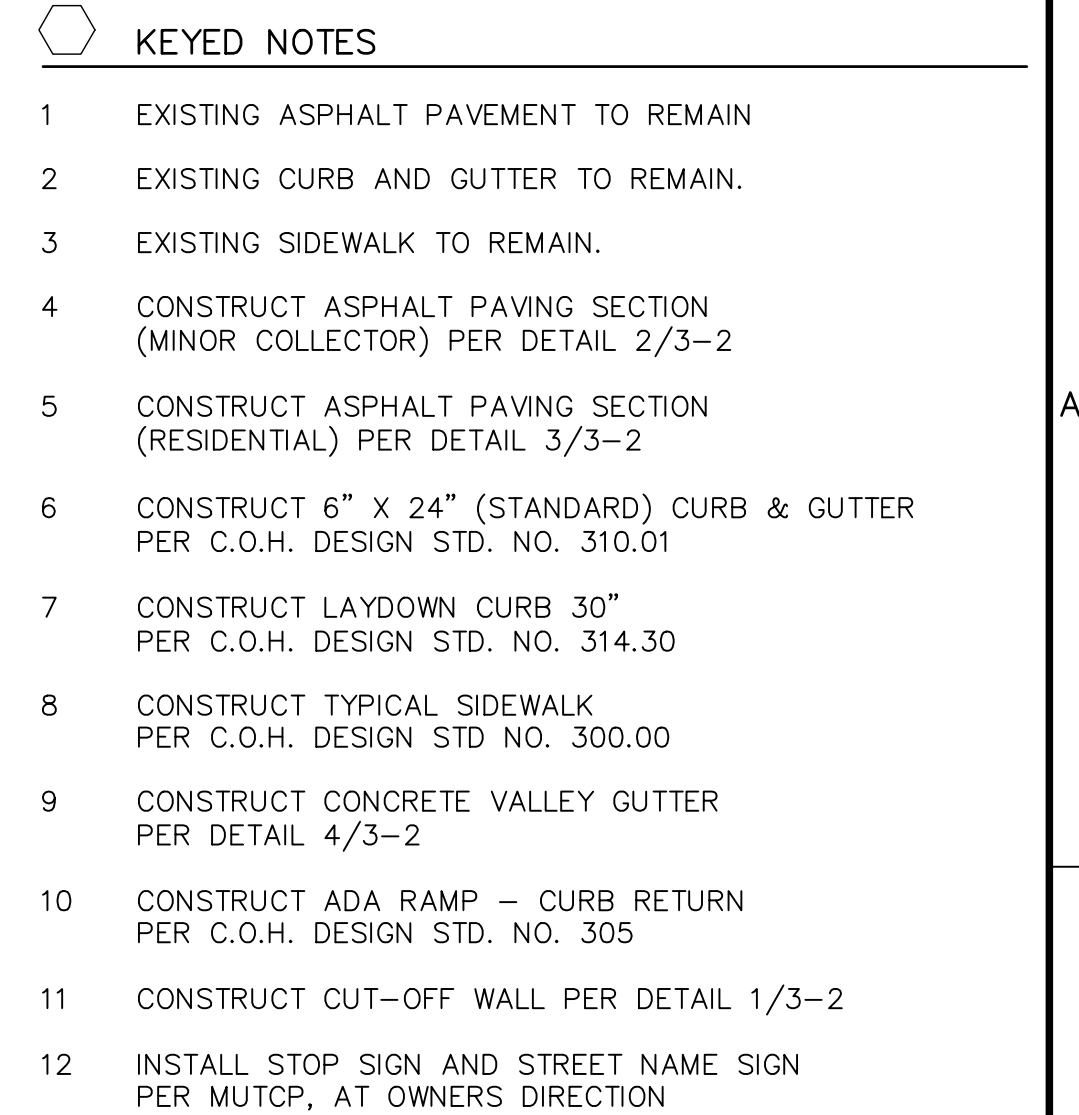
W COLLEGE LN

DESIGNED: RJF							 Fierro & Company ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930	CLIENT:  JOHN LEMKE D.B.A. LEMKE DEVELOPMENT, INC.	MEADOWLANDS UNIT II		SHEET 19 of 31
DRAWN: RJS									SET 4-4		
CHECKED: RJF									LILY POND RD ROADWAY PLAN & PROFILE	DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION						PROJ 21068	

[illegible]

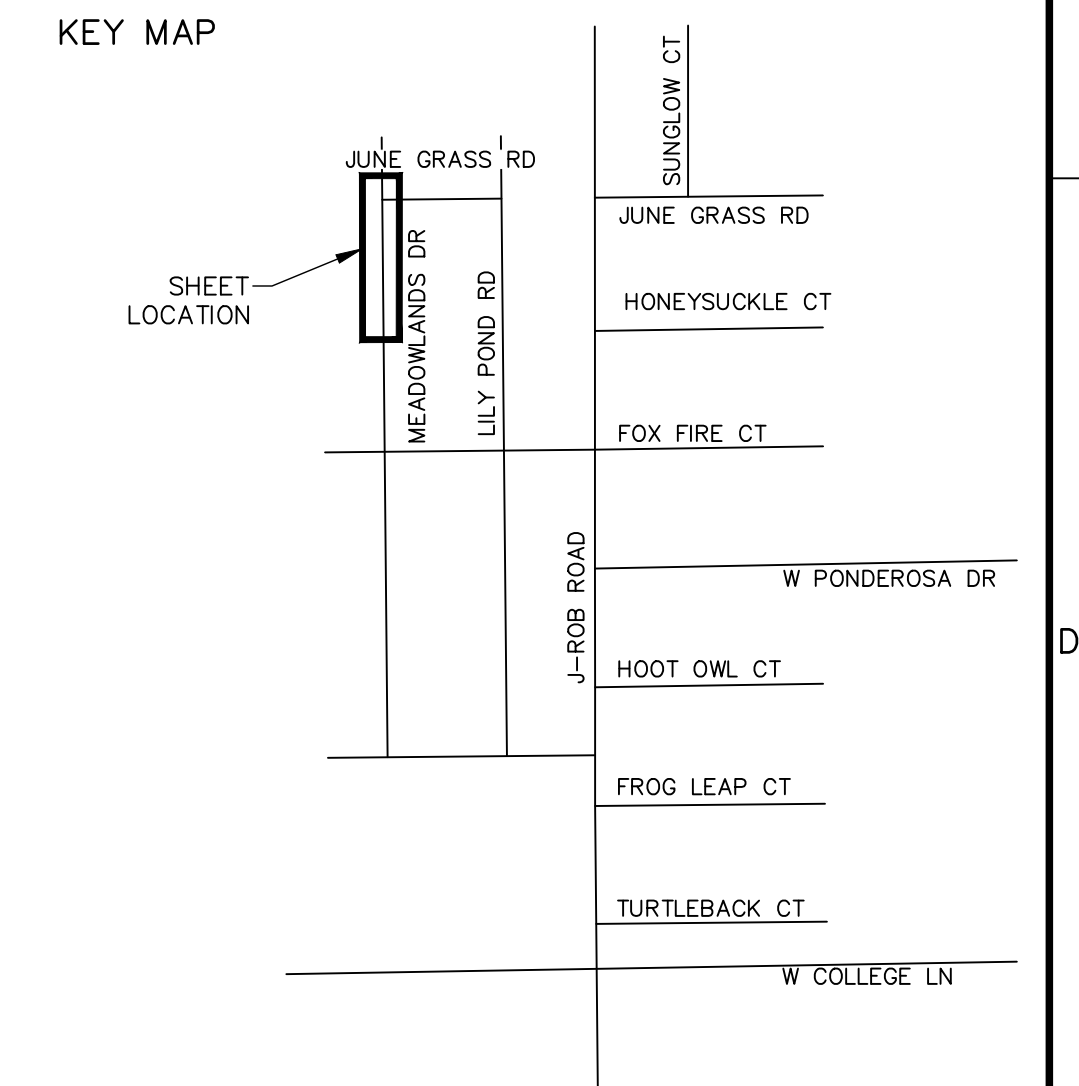
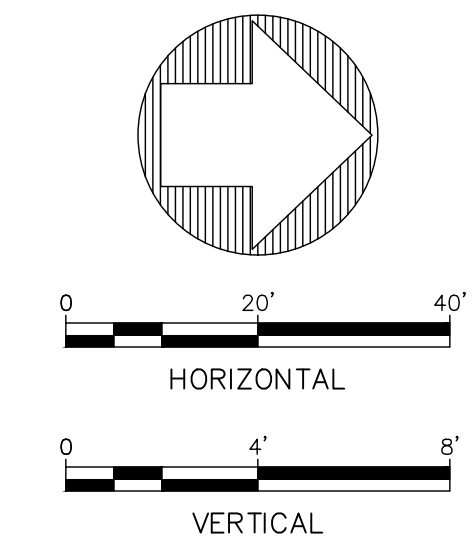
DESIGNED: RJF							<div> <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930</div>	CLIENT:  <div>JOHN LEMKE D.B.A. LEMKE DEVELOPMENT, INC</div>	MEADOWLANDS UNIT II	SHEET 20 of 31
DRAWN: RJS					SET 4-5					
CHECKED: RJF					MEADOWLANDS DR				DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION	ROADWAY PLAN & PROFILE				PROJ 21068	



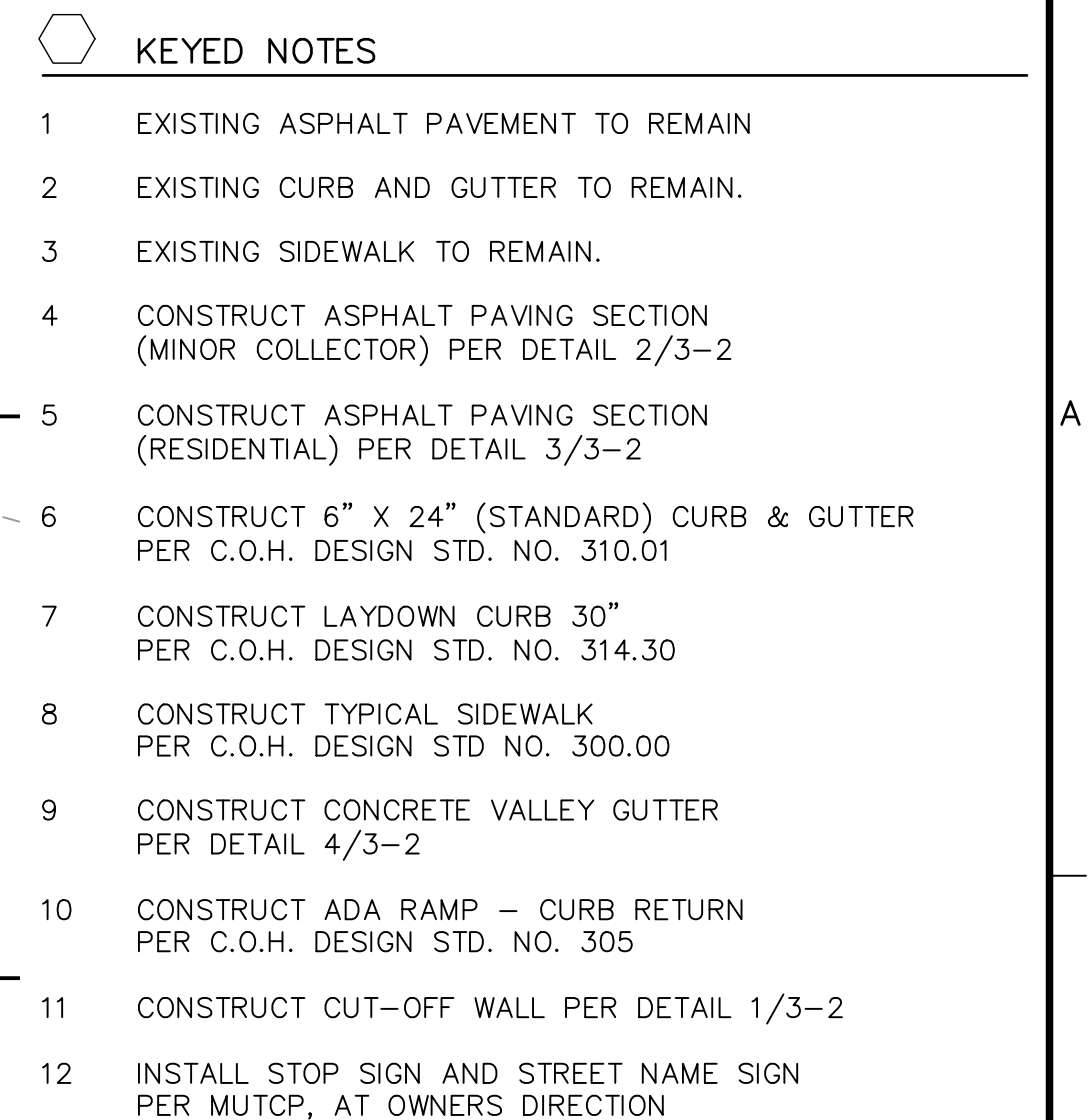


The profile view displays the vertical alignment of a road segment. The horizontal axis represents stationing, ranging from 5+00 to 8+00. The vertical axis represents elevation in feet, ranging from 3650 to 3680. The existing ground surface is shown as a dashed line, and the proposed finished grade is shown as a solid line. The finished grade is a straight line with a -0.20% slope. Key points on the profile include the vertical curve data at station 6+48.50 and the stationing of the existing grade at station 7+48.50.

Station	Existing Grade Elevation (ft)	Finished Grade Elevation (ft)
5+00	3662.89	3662.89
5+25	3663.21	3663.21
5+50	3663.54	3663.54
5+75	3663.83	3663.83
6+00	3664.35	3664.35
6+25	3664.49	3664.49
6+50	3664.52	3664.52
6+75	3664.63	3664.63
7+00	3664.81	3664.81
7+25	3664.48	3664.48
7+50	3663.81	3663.81
7+75	3663.45	3663.45
8+00	3663.08	3663.08

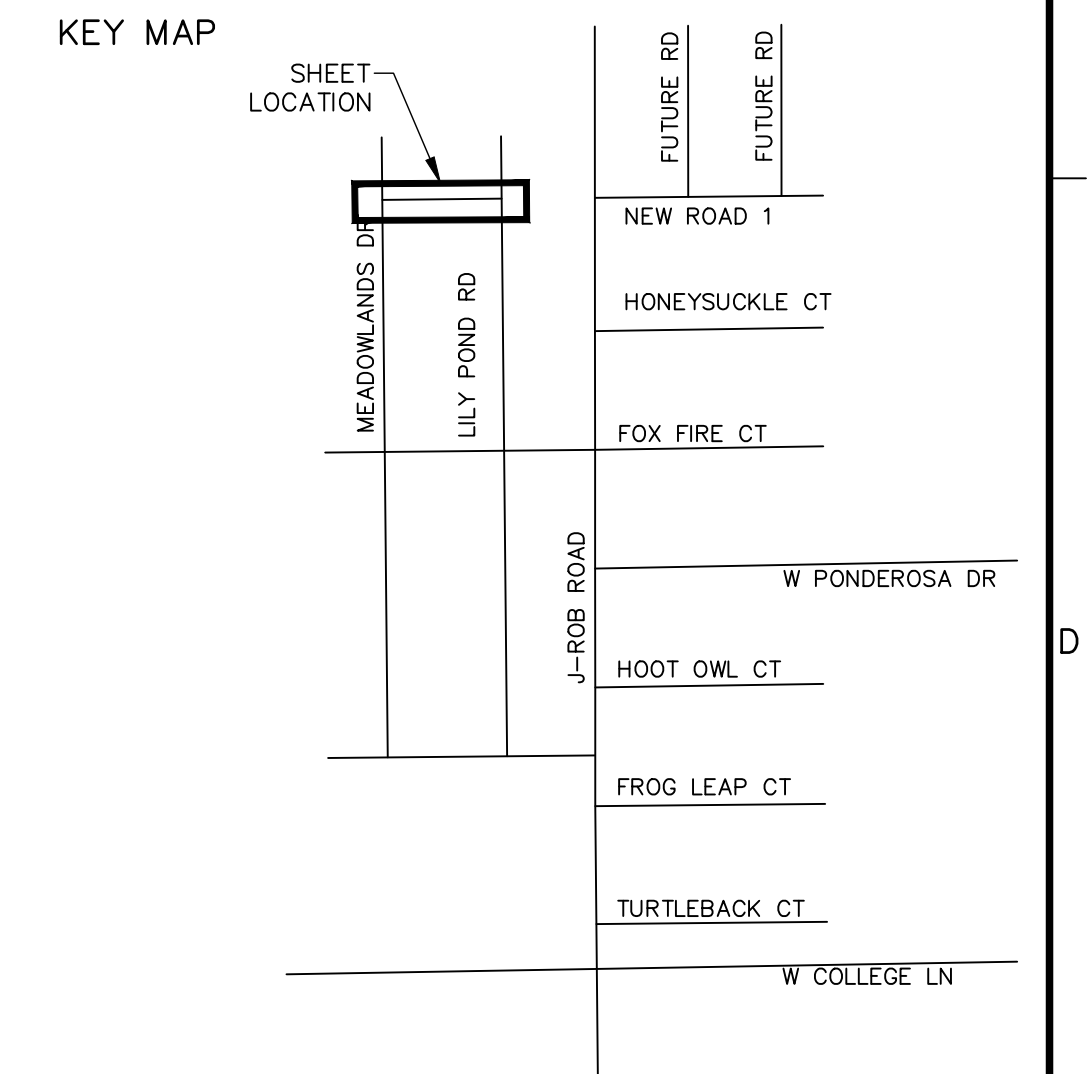
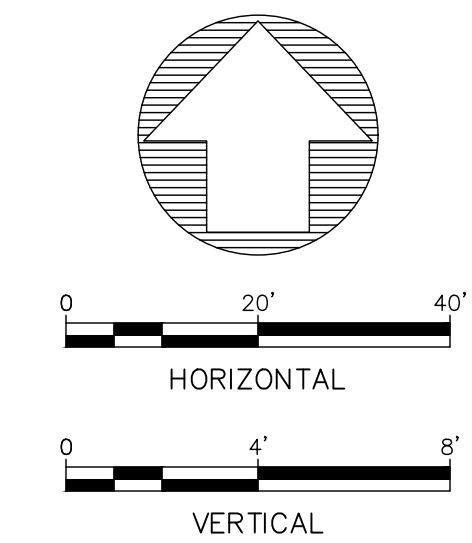



DESIGNED: RJF																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																										
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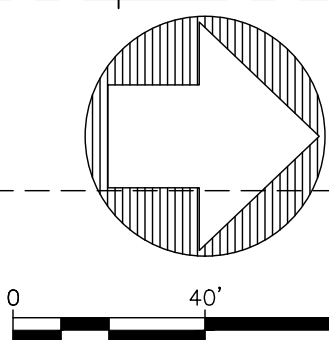
The profile view displays the vertical alignment of a road section. The horizontal axis represents stationing, ranging from 1+00 to 2+00. The vertical axis represents elevation in feet, ranging from 3650 to 3680. The profile shows the existing ground surface (dashed line) and the proposed finished grade (solid line). The finished grade is a smooth curve that follows the general trend of the existing grade, with a slight dip in the center. The profile is divided into segments by vertical curve points, which are labeled with stationing and elevation data. The existing grade is labeled 'EXISTING GRADE' and the finished grade is labeled 'FINISHED GRADE'.



Station	Existing Grade Elevation (ft)	Finished Grade Elevation (ft)
0+16.00	3663.15	3663.15
0+21.00	3663.15	3663.15
0+48.50	3663.56	3663.56
2+16.00	3662.24	3662.24
2+18.00	3662.24	3662.24
2+48.50	3662.58	3662.58

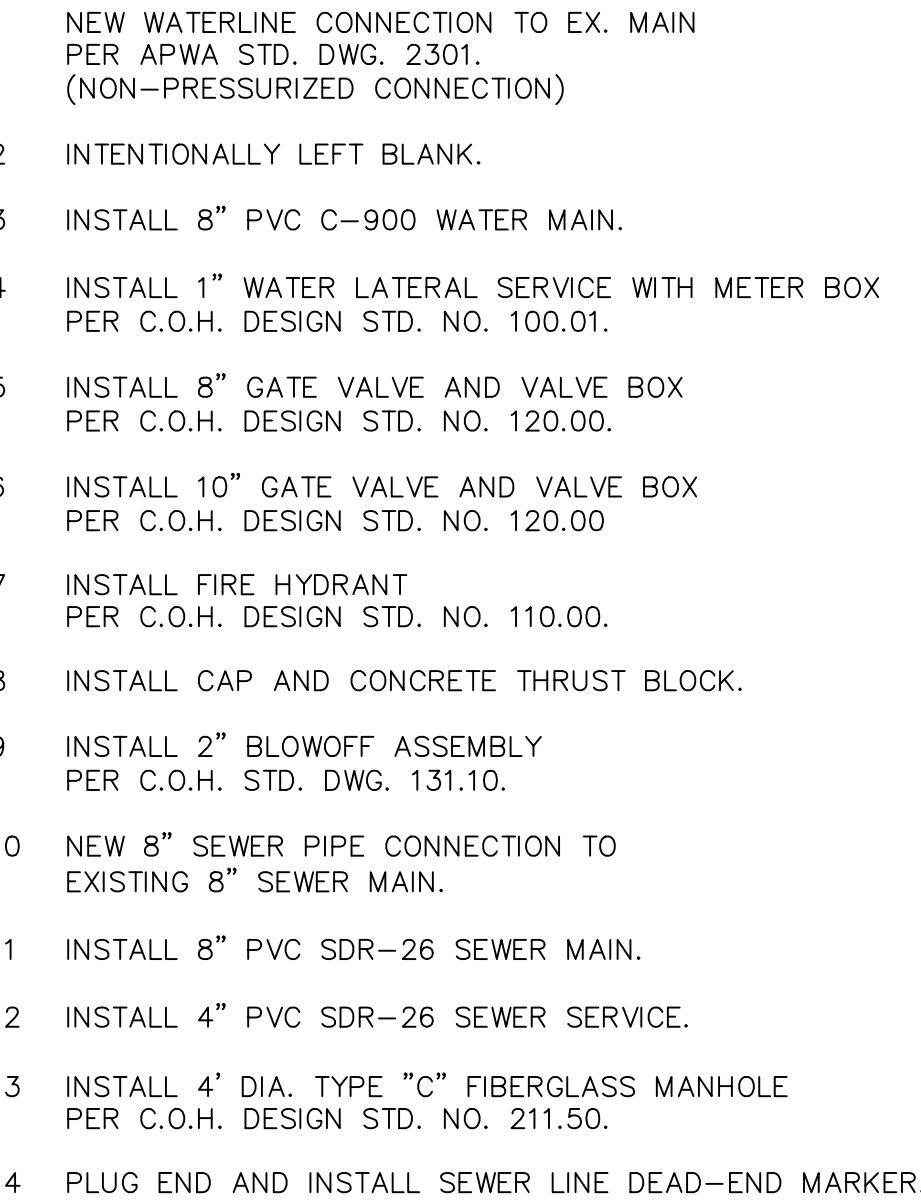


DESIGNED: RJF							 Fierro & Company ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930	CLIENT:  JOHN LEMKE D.B.A. LEMKE DEVELOPMENT, INC.	MEADOWLANDS UNIT II	SHEET 22 of 31
DRAWN: RJS										SET 4-7
CHECKED: RJF									JUNE GRASS RD ROADWAY PLAN & PROFILE	DATE Dec-2022
APPROVED: ###	NO.	DATE	BY	REVISION						PROJ 21068





DESIGNED: RJF							 <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C Albuquerque, NM 87107 (505) 352-8930	CLIENT:  <b>JOHN LEMKE</b> <b>D.B.A.</b> <b>LEMKE DEVELOPMENT, INC.</b>	<b>MEADOWLANDS</b> <b>UNIT II</b>	SHEET 23 of 31
DRAWN: JB									SET 5-0	
CHECKED: RJF									<b>OVERALL UTILITY PLAN</b>	DATE Dec-2022
APPROVED: ###	NO.	DATE	BY	REVISION						PROJ 21068



MEADOWLANDS DR

JUNE GRASS RD

LILY POND RD

HONEYSUCKLE CT

FOX FIRE CT

J-ROB ROAD

W PONDEROSA DR

HOOT OWL CT

FROG LEAP CT

TURTLEBACK CT

W COLLEGE LN

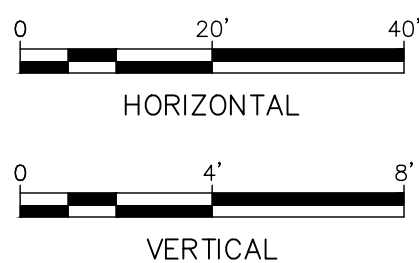
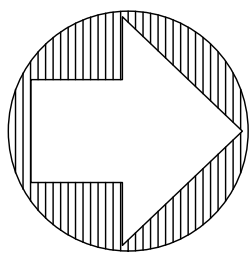
SHEET LOCATION

SHEET	24	of	31
SET	5-1		
DATE	Dec-2022		
PROJ	21068		

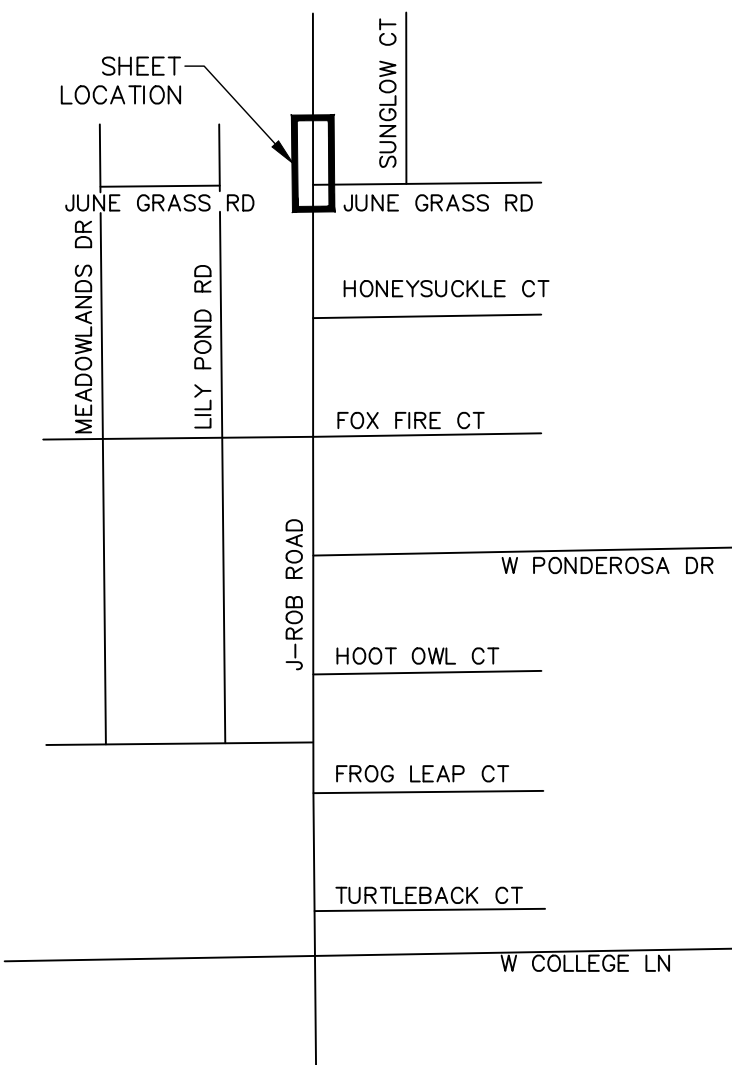




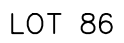
- NEW WATERLINE CONNECTION TO EX. MAIN  
PER APWA STD. DWG. 2301.  
(NON-PRESSURIZED CONNECTION)
- INTENTIONALLY LEFT BLANK.
- INSTALL 8" PVC C-900 WATER MAIN.
- INSTALL 1" WATER LATERAL SERVICE WITH METER BOX  
PER C.O.H. DESIGN STD. NO. 100.01.
- INSTALL 8" GATE VALVE AND VALVE BOX  
PER C.O.H. DESIGN STD. NO. 120.00.
- INSTALL 10" GATE VALVE AND VALVE BOX  
PER C.O.H. DESIGN STD. NO. 120.00
- INSTALL FIRE HYDRANT  
PER C.O.H. DESIGN STD. NO. 110.00.
- INSTALL CAP AND CONCRETE THRUST BLOCK.
- INSTALL 2" BLOWOFF ASSEMBLY  
PER C.O.H. STD. DWG. 131.10.
- NEW 8" SEWER PIPE CONNECTION TO  
EXISTING 8" SEWER MAIN.
- INSTALL 8" PVC SDR-26 SEWER MAIN.
- INSTALL 4" PVC SDR-26 SEWER SERVICE.
- INSTALL 4" DIA. TYPE "C" FIBERGLASS MANHOLE  
PER C.O.H. DESIGN STD. NO. 211.50.
- PLUG END AND INSTALL SEWER LINE DEAD-END MARKER.



## KEY MAP



DESIGNED: RJF									<div><div><div>FC</div></div><div>Fierro &amp; Company</div><div>ENGINEERING   SURVEYING</div><div>3201 4th STREET NW, SUITE C</div><div>ALBUQUERQUE, NM 87107</div><div>(505) 352-8930</div></div>	CLIENT: <div>JOHN LEMKE</div> <div>D.B.A.</div> <div>LEMKE DEVELOPMENTS, INC.</div>	MEADOWLANDS UNIT II	SHEET 25 of 31
DRAWN: RJS							SET 5-2					
CHECKED: RJF											DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION							PROJ 21068	
												JA-ROB RD UTILITY PLAN & PROFILE

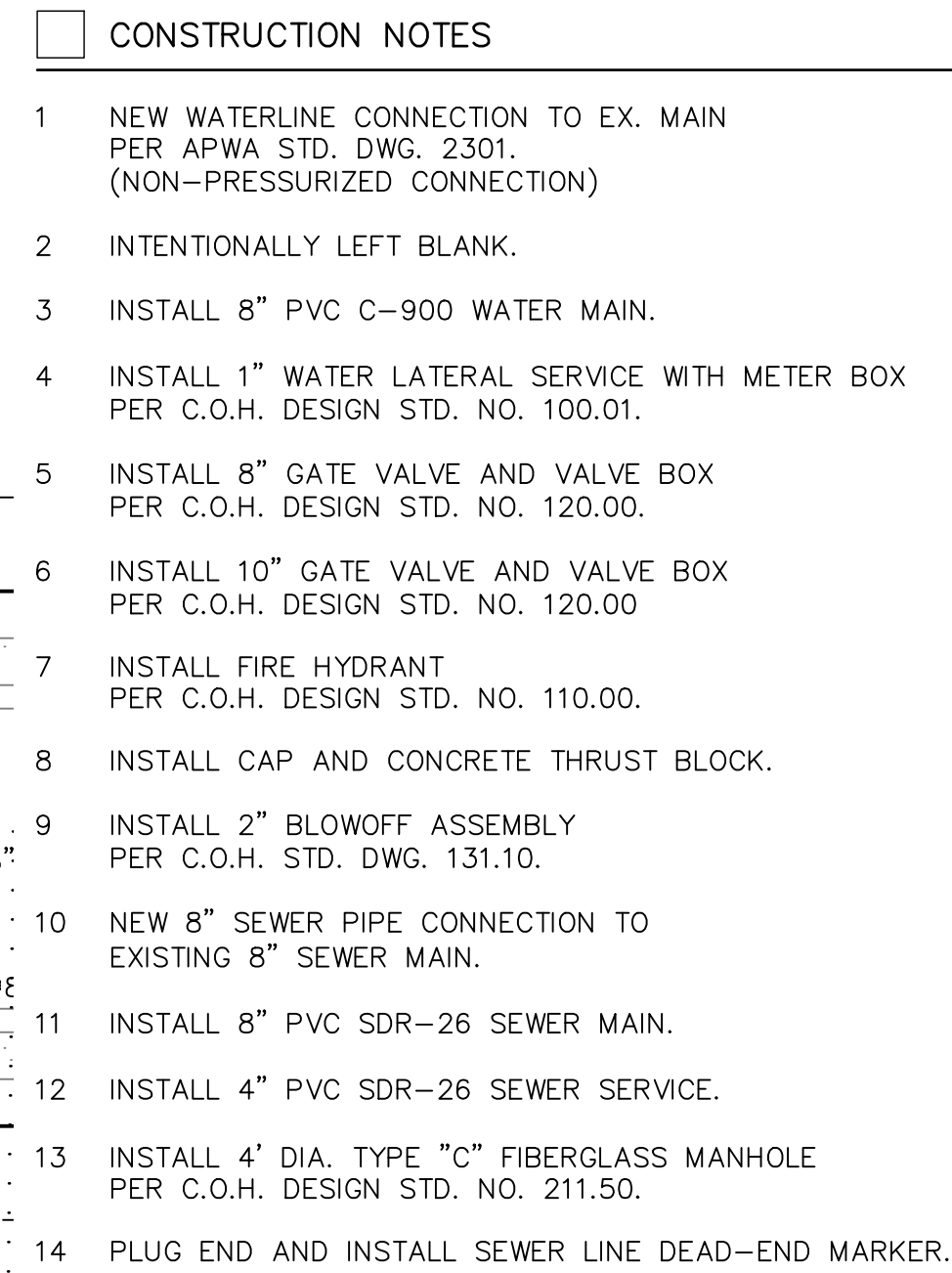


SHEET	26	of	31
SET	5-3		
DATE	Dec-2022		
PROJ	21068		





SHEET	27 of 31
SET	5-4
DATE	Dec-2022
PROJ	21068



Profile view of sewer and water lines. The vertical axis shows elevation in feet, ranging from 3650 to 3680. The horizontal axis shows stationing from 0+00 to 4+00.

**Existing Sewer Manhole (MH) at Station 0+00.00:**

- STA: 0+00.00 OFF: 0.00
- RIM: 3662.36
- INV IN: 3656.20 8"
- INV IN: 3656.20 8"
- INV OUT: 3656.20 8"
- INV OUT: 3656.20 8"

**Existing Sewer Line:**

- 104.5' of 8" PVC SEWER PIPE (SDR 26) @ 0.22%
- INV. ELEV: 3656.71 (MATCH EXISTING)

**New Sewer Manhole #3 at Station 2+72.50:**

- NEW SEWER MH #3
- 4' DIA. TYPE "E"
- STA: 2+72.50 OFF: 0.00
- RIM: ~~3662.85~~ 3663.26
- INV IN: ~~3656.94~~ 8" PVC Pipe 3656.81
- INV OUT: ~~3656.94~~ 8" PVC Pipe 3656.75

**New Sewer Line:**

- 311.5' of 8" PVC SEWER PIPE (SDR 26) @ 0.22%

**Water Line:**

- 8" PVC WATERLINE

**Grades:**

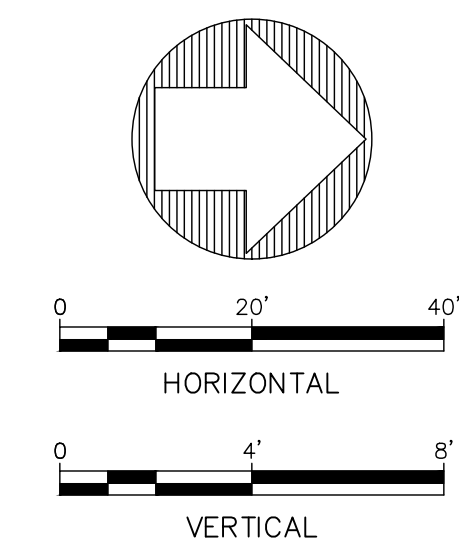
- EXISTING GRADE
- FINISHED GRADE

**Other Labels:**

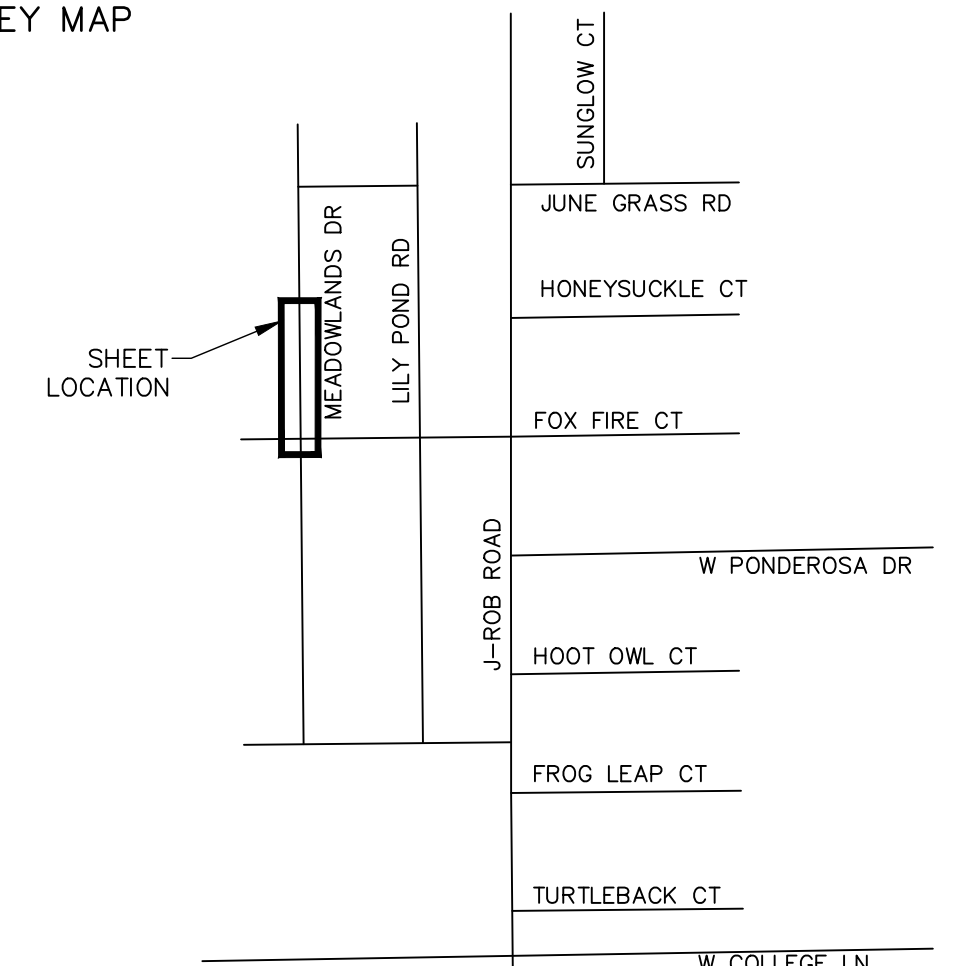
- EX. 8" WATERLINE
- EX. 8" SAS
- 3' MIN
- 0.20%



**Elevation Data (from profile view):**

Station	Elevation (ft)
3662.881	3662.881
3663.169	3663.169
3663.258	3663.258
3663.288	3663.288
3663.317	3663.317
3663.316	3663.316
3663.305	3663.305
3663.295	3663.295
3662.62	3662.62
3663.284	3663.284
3662.66	3662.66
3663.199	3663.199
3662.70	3662.70
3663.048	3663.048
3662.74	3662.74
3662.896	3662.896
3662.78	3662.78
3662.637	3662.637
3662.82	3662.82
3662.563	3662.563
3662.86	3662.86
3662.554	3662.554
3662.90	3662.90
3662.468	3662.468
3662.94	3662.94
3662.362	3662.362
3662.98	3662.98
3662.367	3662.367
3663.02	3663.02
3662.361	3662.361
3663.06	3663.06
3662.361	3662.361
3663.10	3663.10
3662.362	3662.362
3663.14	3663.14
3662.362	3662.362



## KEY MAP



DESIGNED: RJF										 Fierro & Company ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930	CLIENT:  JOHN LEMKE D.B.A. LEMKE DEVELOPMENT, INC	MEADOWLANDS UNIT II	SHEET 28 of 31
DRAWN: RJS								SET 5-5					
CHECKED: RJF								MEADOWLANDS DR UTILITY PLAN & PROFILE				DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION								PROJ 21068	

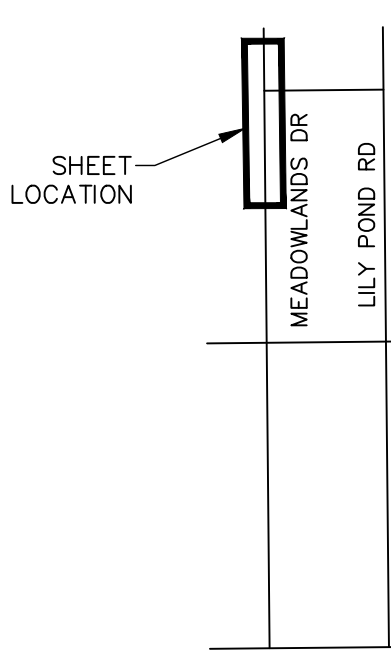




- A

3

3

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HONEY  
FOX  
HOOT  
FROG  
TURTLE

SUNGLOW CT

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HOOT

EROC

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TURTI

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# F

[illegible]

SHEET 29 of 31

SET 5-6

DATE      Dec-2022

PROJ 21068

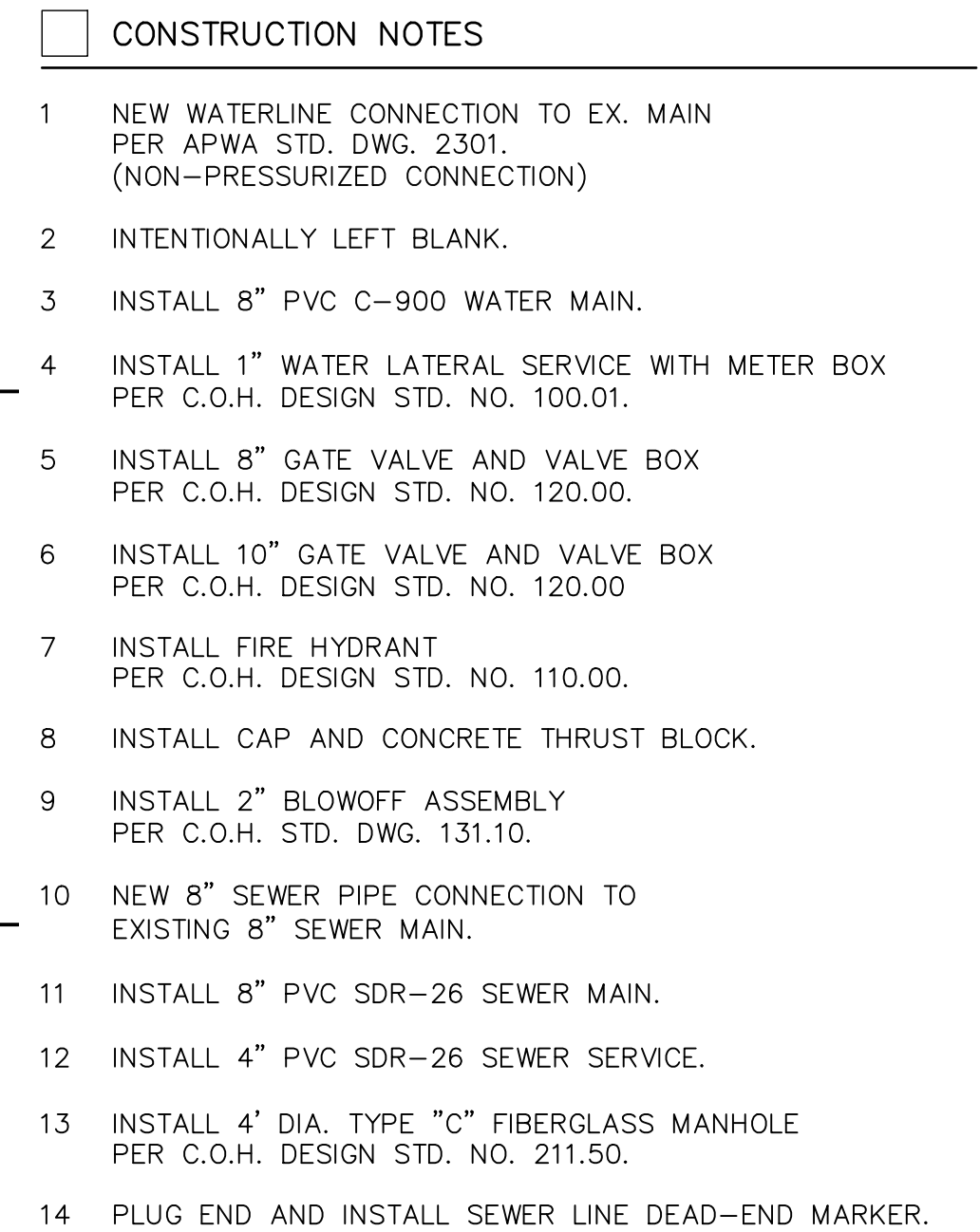
**CLIENT:**

**JOHN LEMKE  
D.B.A.  
LEMKE DEVELOPMENT, INC**

**MEADOWLANDS  
UNIT II**

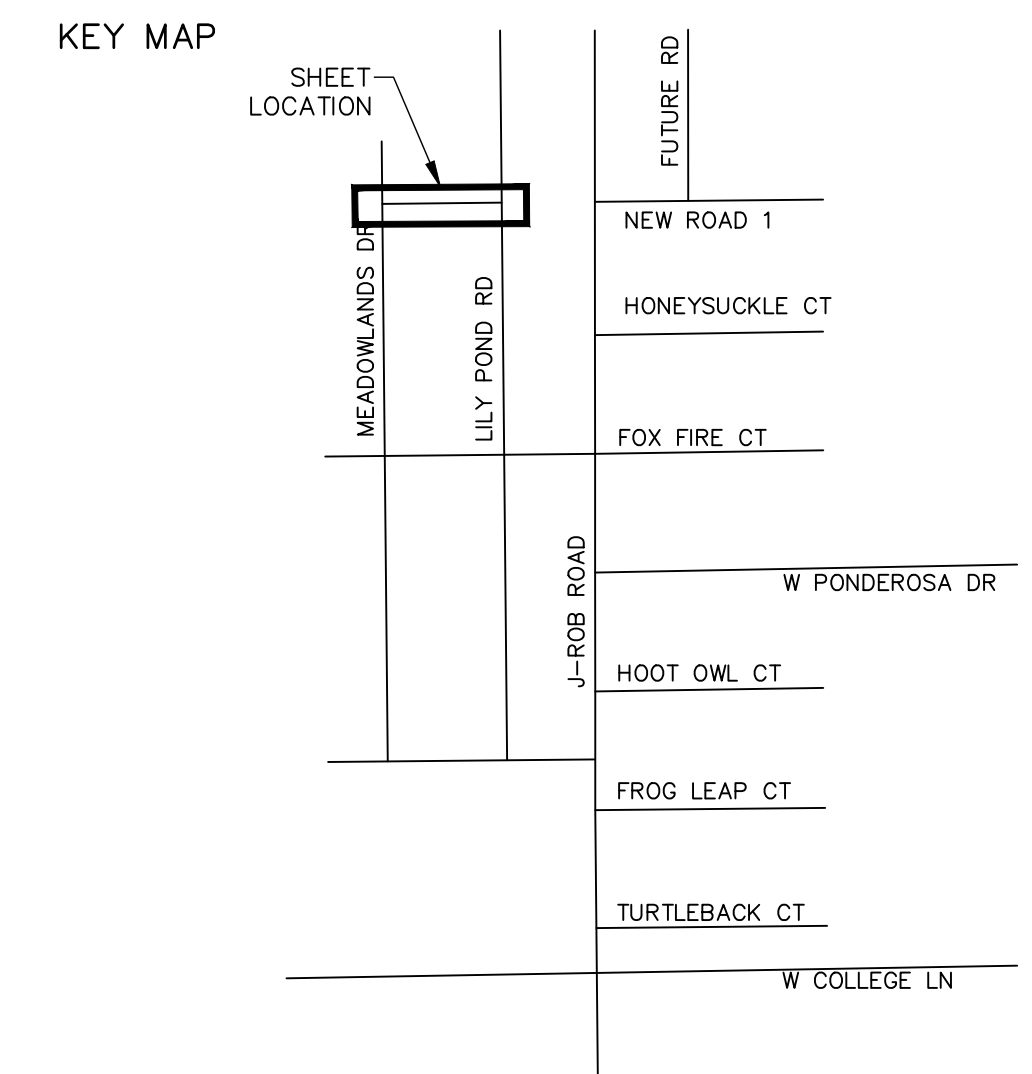
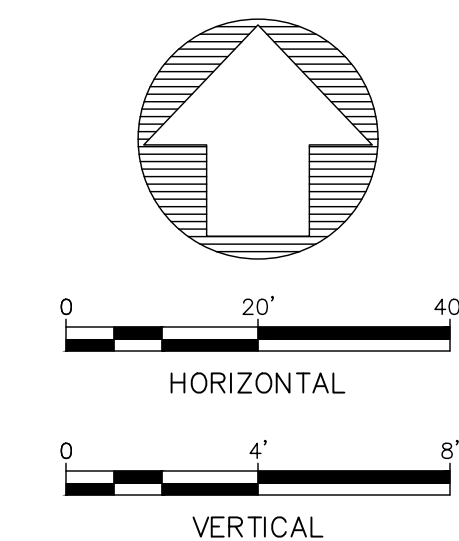
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**MEADOWLANDS DR  
UTILITY PLAN & PROFILE**



Profile view of sewer and water lines. The vertical axis shows elevation in feet (3650 to 3680). The horizontal axis shows stationing (1+00 to 2+00). The profile includes existing and finished ground lines, a 6" PVC waterline, and a 235.7' of 8" PVC sewer pipe (SDR 26) @ 0.22% slope. Two new sewer manholes (MH #5 and MH #2) are shown. MH #5 is at station 0+61.33, and MH #2 is at station 2+97.00. The sewer pipe is 8" diameter, type "E", with an invert of 3657.31 at MH #5 and 3656.79 at MH #2. The waterline is 6" diameter, with an invert of 3658.81 at MH #5 and 3656.67 at MH #2. The sewer pipe is 235.7' long, with an invert of 3657.31 at MH #5 and 3656.79 at MH #2. The waterline is 6" diameter, with an invert of 3658.81 at MH #5 and 3656.67 at MH #2. The sewer pipe is 235.7' long, with an invert of 3657.31 at MH #5 and 3656.79 at MH #2. The waterline is 6" diameter, with an invert of 3658.81 at MH #5 and 3656.67 at MH #2.



Station	Existing Grade (ft)	Finished Grade (ft)	6" PVC Waterline Invert (ft)	8" PVC Sewer Pipe Invert (ft)
0+00	3664.58	3663.131	3663.131	3663.131
0+25	3664.64	3663.436	3663.436	3663.436
0+61.33 (MH #5)	3664.44	3663.511	3663.511	3657.31
1+00	3664.24	3663.420	3663.420	3657.31
1+25	3664.05	3663.329	3663.329	3657.31
1+50	3663.92	3663.238	3663.238	3657.31
1+75	3663.81	3663.147	3663.147	3657.31
2+00	3663.68	3663.056	3663.056	3656.79
2+25	3663.53	3662.965	3662.965	3656.79
2+50	3663.29	3662.874	3662.874	3656.79
2+75	3663.03	3662.783	3662.783	3656.79
3+00	3662.76	3662.692	3662.692	3656.79
3+25	3662.55	3662.481	3662.481	3656.79
3+50	3662.38	3662.221	3662.221	3656.79
3+97.00 (MH #2)	3662.02	3662.02	3662.02	3656.79



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DRAWN: RJS					SET 5-7					
CHECKED: RJF					<div>JUNE GRASS RD UTILITY PLAN &amp; PROFILE</div>				DATE Dec-2022	
APPROVED: ###	NO.	DATE	BY	REVISION	PROJ 21068					





DESIGNED: RJF								<div> <b>Fierro &amp; Company</b> ENGINEERING   SURVEYING 3201 4th. STREET NW, SUITE C ALBUQUERQUE, NM 87107 (505) 352-8930</div>	CLIENT: <div><b>JOHN LEMKE</b> <b>D.B.A.</b> <b>LEMKE DEVELOPMENT, INC.</b></div>	<b>MEADOWLANDS UNIT II</b>	SHEET 31 of 31
DRAWN: JB						SET 6-0					
CHECKED: RJF											DATE Dec-2022
APPROVED: ###	NO.	DATE	BY	REVISION							PROJ 21068